City of Plymouth Plan Commission

Members: Please notify us if you are unable to attend the meeting.

Chairman; Mayor Don Pohlman

Members:
Angie Matzdorf (Council Representative)
Jane Meyer
Jeremy Schellin
Ron Nicolaus
John Wyatt
Justin Schmitz

The City of Plymouth Plan Commission will have a meeting on **Thursday September 4, 2025 at** 6:00 PM in **Room 305**, City Hall, and Plymouth, WI.

The agenda will be as follows:

1.) Call to Order & Roll Call

2.) Approval of Minutes from August 7, 2025

3.) Site Plan Seeking Approval; 615 E. Main St, Jonathon & Samantha Puetz, for the construction of a 432 square foot detached garage. Puetz (enclosure)

4.) Site Plan Seeking Approval; 3950 CTH PP, JJ Coolers LLC, for additional parking lot space and installation of a gravel trailer parking area. Keller, Inc. (enclosure)

5.) Site Plan Seeking Approval; Parcel number 59271822750 (southeast corner of STH 57 and CTH PP), Sargento Cheese Inc, regarding minor revisions to previously approved site plans for new ~384,000 square foot industrial facility in the H-I Heavy Industrial zoning district.

6.) Traffic Impact Analysis Review and Approval; For the new Sargento facility on the corner of STH 57 & CTH PP. Review and approval of TIA by Plan Commission required as part of contingent approval of site plan received at July 10, 2025 Plan Commission meeting.

7.) Concept Plan Seeking Feedback; Parcel Number 59271829210, located on Section 33 along State Highway 67 on the southwest side of the City of Plymouth, regarding a concept plan for a single-family residential subdivision plat and multi-family residential project. Neumann Developments, Inc. (enclosure)

8.) Fee Schedule Update: Staff seeking recommendation to update the Zoning Fee Schedule last updated in 2020

9.) Communication – Letters, E-mails, or reports Related to the Plan Commission (Chairman, Secretary, Plan Commission Members, City of Plymouth Staff/Alderpersons)

10.) Adjournment

It is possible that members of and possibly a quorum of members of other governmental bodies of the municipality may be in attendance at the above stated meeting to gather information. No action will be taken by any governmental body at the above stated meeting other than the governmental body specifically referred to above in this notice. Please note that, upon reasonable notice, efforts will be made to accommodate the needs of disabled individuals through appropriate aids and services. For additional information or to request this service, contact City of Plymouth ADA Coordinator Leah Federwisch, located in the Plymouth Utilities office at 900 County Road PP, Plymouth, WI or call 920-893-3853

City of Plymouth Plan Commission

UNOFFICIAL MINUTES

Mayor Pohlman called the meeting to order at 6:00 PM on August 7, 2025. The following members were present: Jane Meyer, Ron Nicolaus, John Wyatt, Greg Hildebrand (arrived at 6:08 pm), and Justin Schmitz. Also present: City Administrator/Utilities Manager Tim Blakeslee, Assistant City Administrator/Community Development Director Jack Johnston, Fire Chief Ryan Pafford

1.) **Call to Order & Roll Call**
Mayor Pohlman called the meeting to order at 6:00 pm. The meeting began with a roll call of the Plan Commission.

2.) **Approval of Minutes from July 10, 2025:** Motion was made by Nicolaus/Schmitz to approve the minutes. Upon the call of the roll, all voted aye. Motion carried.

3.) **Recommendation of Extraterritorial Certified Survey Map; Parcel numbers 59016217350 and 59016217360 – located on Sandstone Lane in the Town of Plymouth. Rortvedt Trust (enclosure)**
Cheryl Rortvedt, on behalf of the Rortvedt Trust, has submitted a certified survey map (CSM) for extraterritorial review and approval by the City of Plymouth due to the property being within the 1.5 mile extraterritorial review area of the City of Plymouth. The property itself is located within the Town of Plymouth and received Town approval on July 8, 2025.

Cheryl Rortvedt and Ed Harvey were present to answer any questions of the Commission.

Motion made by Nicolaus/Wyatt to recommend approval of the extraterritorial CSM as presented. Upon the call of the roll, all voted aye. Motion carried.

4.) **Site Plan Seeking Approval; 216 Western Ave, William Brin, for the installation of a 77 square foot shed. Brin (enclosure)**
William Brin has submitted an application to install a 7’ by 11’ storage shed on the northeast corner of 216 Western Ave. B-2 zoning does not have setbacks, so the placement of the shed is conforming. The property is used as a single-family home. The Plan Commission does not typically review additions or new constructions for residential properties, but as the property’s base zoning is B-2 general business (housing is a permitted use), a site plan review is required by City code.

It should be noted the shed is prefab and was installed prior to Plan Commission approval/building permit issuance. Speaking with the building permit office, it is common for prefab sheds to be installed by homeowners and permits be issued afterwards.

William Brin was present to answer any questions of the Commission.

Motion made by Schmitz/Nicolaus to approve the site plan for 216 Western Ave, William Brin, for the installation of a 77 square foot shed. Upon the call of the roll, all voted aye. Motion carried.

5.) **Site Plan Seeking Approval; 615 E. Main St, Jonathon & Samantha Puetz, for the construction of a 432 square foot detached garage. Puetz (enclosure)**
This item was tabled due to non-attendance by the applicant.

6.) Site Plan Seeking Approval; 716-744 N. Pleasant View Road, Fairview Crossing Apartment Association LP, for the construction of a new clubhouse and parking lot expansion. Cityscape Architecture (enclosure)

Fairview Crossings Apartments, located along Valley Road and N. Pleasant View Road, have applied for a new clubhouse, expanded parking lot, and a new driveway access. The clubhouse will be 2,634 square feet and feature a gathering room, activity room, two offices, bathrooms, site storage, and a trellised patio area.

An existing detached garage will be removed, and a new asphalted parking lot will be installed featuring approximately 16 new parking spaces adjacent to the new clubhouse. The clubhouse siding will match the existing residential buildings on site. The site plan also shows a new driveway access off of Valley Road adjacent to the new parking lot for the clubhouse.

Greg Schumacher was present on behalf of Cityscape Architecture to answer any questions. Fire Chief Pafford commented the second driveway access would help emergency vehicles gain access in the case of an emergency.

Commissioner Nicolaus asked if the existing garage had fire protection. Mr. Schumacher commented that it was not required to as it only housed tool, not vehicles.

Motion made by Nicolaus/Wyatt to approve the site plan for 716-744 N. Pleasant View Road, Fairview Crossing Apartment Association LP, for the construction of a new clubhouse and parking lot expansion. Upon the call of the roll, all voted aye. Motion carried.

7.) Recommendation of City of Plymouth Certified Survey Map; Parcel number 59271827950 – located on Kiley Way and Walton Drive in the City of Plymouth, to divide the parcel into two parcels of 1.81 acres and 1.4 acres in the B-1 Business Office zoning district. La Macchia Group/Excel (enclosure)

La Macchia Group, on behalf of their client Premier Financial Credit Union, has submitted a site plan review application for a new 2,909 square foot credit union building on the corner of Kiley Way and Walton Drive.

As part of the project, Excel Engineering has also submitted a two-lot certified survey map (CSM) to divide the current ~3.21 acre parcel into two parcels: a 1.4 acre parcel that will be occupied by Premier Financial, and another 1.81 acre parcel that will remain vacant for future development. The new lots will maintain the existing B-1 Business Office zoning that the existing parcel has.

Reid Jahns and Eric Manders of Excel Engineering were present to answer any questions.

Assistant City Administrator Johnston noted that item #7 and #8 could be discussed together but should approved/denied via separate motions.

Mr. Manders noted the site plan showing a connection to the private parking lot to the west was contingent on reaching an agreement with the owner for an access easement. The Commission discussed this, and was comfortable with approving the site plan with or without the access. Fire Chief Pafford noted the existing access was sufficient and the even grade at the location would allow emergency vehicles to travel over the grass from the existing parking lot to the site if need be.

Motion by Nicolaus/Wyatt to approve the CSM for Parcel number 59271827950 – located on Kiley Way and Walton Drive in the City of Plymouth, to divide the parcel into two parcels of 1.81 acres and 1.4 acres in the B-1 Business Office zoning district. Upon the call of the roll, all voted aye. Motion carried.

8.) Site Plan Seeking Approval; Parcel number 59271827950, to construct a new 2,909 square foot financial institution to be occupied by Premier Financial Credit Union. Property to be located on the corner of Kiley Way and Walton Drive. La Macchia Group/Excel (enclosure)

This item continues from item #7. Motion by Wyatt/Schmitz to approve the site plan for Parcel number 59271827950, to construct a new 2,909 square foot financial institution to be occupied by Premier Financial Credit Union. Property to be located on the corner of Kiley Way and Walton Drive. Upon the call of the roll, all voted aye. Motion carried.

9.) Site Plan Seeking Approval; 4200 CTH PP, SFC-Plymouth, LLC (dba Sartori Cheese) for the construction of a ~68,000 square foot building addition and related site plan improvements. Abacus Architects (enclosure)

SFC-Plymouth, LLC (dba Sartori Cheese) has submitted a site plan review application for a new ~68,000 square foot building addition to accommodate additional converting and packaging lines at their facility located at 4200 CTH PP in the City of Plymouth. The addition will be built and connect to the existing building and previous addition on the property's eastern boundary along Sartori Whey. The addition will be served by existing driveways located off of Sartori Whey, with primary employee parking to remain located off of CTH PP.

City Administrator Blakeslee noted the site plan can be approved contingent on an approved utility plan related to the location of a manhole.

Motion by Nicolaus/Schmitz to approve the site plan for Sartori for the construction of a ~68,000 square foot building addition and related site plan improvements, contingent on a final utility plan. Upon the call of the roll, all voted aye. Motion carried.

10.) Communication – Letters, E-mails, or reports Related to the Plan Commission (Chairman, Secretary, Plan Commission Members, City of Plymouth Staff/Alderpersons)

Assistant City Administrator Johnston reported the City Development page is live on the City website and staff will work on updating monthly and improving it. He encouraged the Plan Commission to view it and provide feedback.

The Mayor noted this would be Alderman Hildebrand's last meeting as a member of the Plan Commission. He thanked Alderman Hildebrand for his service to the City.

11.) Adjournment

Vice Chairman Hildebrand asked for a motion to adjourn the meeting. Motion was made by Wyatt/Schmitz to adjourn the meeting. A unanimous aye vote was cast. Motion carried.

City of Plymouth
128 Smith St. - P.O. Box 107
Plymouth, WI 53073-0107

Telephone: **(920) 893-3745**
Facsimile: **(920) 893-0183**
Web Site: **plymouthgov.com**

DATE: July 9, 2025

TO: Plan Commission

FROM: Clara Yoder, Administrator Intern

RE: **Site Plan Seeking Approval;** 615 E. Main St, Jonathon Puetz, for the construction of a 432 square foot detached garage. Puetz (enclosure)

ZONING CONSISTENCY MATRIX		
	Required	***Provided***
Zoning	B-2	B-2 (no change)
Maximum Lot Coverage of All Buildings	None	n/a
Minimum Lot Width at Building Line	None	n/a
Minimum Front Yard Setback	None	n/a
Minimum Side Yard Setback **Principal Building** **Accessory Building**	None	n/a
Minimum Rear Yard Setback	None	n/a
Maximum Height Any Structures	Principal: 45' Accessory: 25'	~11.5' for garage (from top of roof)

Background:
Jon Puetz has submitted an application to construct a 18' by 24' storage garage on the southwest corner of 615 East Main Street. The project also calls for an overhang to be 12'' off the west property line and 48'' off south property line to avoid underground power utilities. B-2 zoning does not have setbacks, so the placement of the garage is conforming. The property is used as a single-family home. The Plan Commission does not typically review additions or new constructions for residential properties, but as the property's base zoning is B-2 general business (housing is a permitted use), a site plan review is required by City code.

City Zoning/Comprehensive Plan:
The property is within the B-2 Business General zoning district. According to section 13-1-140 of City Code, the proposed detached garage conforms to the required setbacks for the district. The code indicates that both the existing building and the proposed garage meet the setback requirements. The Plan Commission should note that the B-2 district has specific setback restrictions for principal and accessory buildings, as outlined in city code.

Shoreland Zoning/Wetlands/Floodplain:
The property is not within City shoreland zoning nor an identified or suspected wetland. FEMA flood plain mapping also indicates the property is outside an identified floodplain.

Public Works/Plymouth Fire Department Review:
Both the Public Works Director Austin and Chief Pafford reviewed the proposal and have no concerns.

Plan Commission Consideration(s):
Sec. 15-1-3(g) of the City Code indicates that the Plan Commission must review and approve any construction, reconstruction, expansion or conversion, except for one and two family residences in residentially zoned districts* before the issuance of a building permit from the building inspector:
**As this property has business zoning, a site plan review is still required.*

- *Site plan approval.* All applications for building permits for any construction, reconstruction, expansion or conversion, except for one and two family residences in residentially zoned districts shall require site plan approval by the plan commission.

When considering acting on a site plan application, the Plan Commission should consider the following:

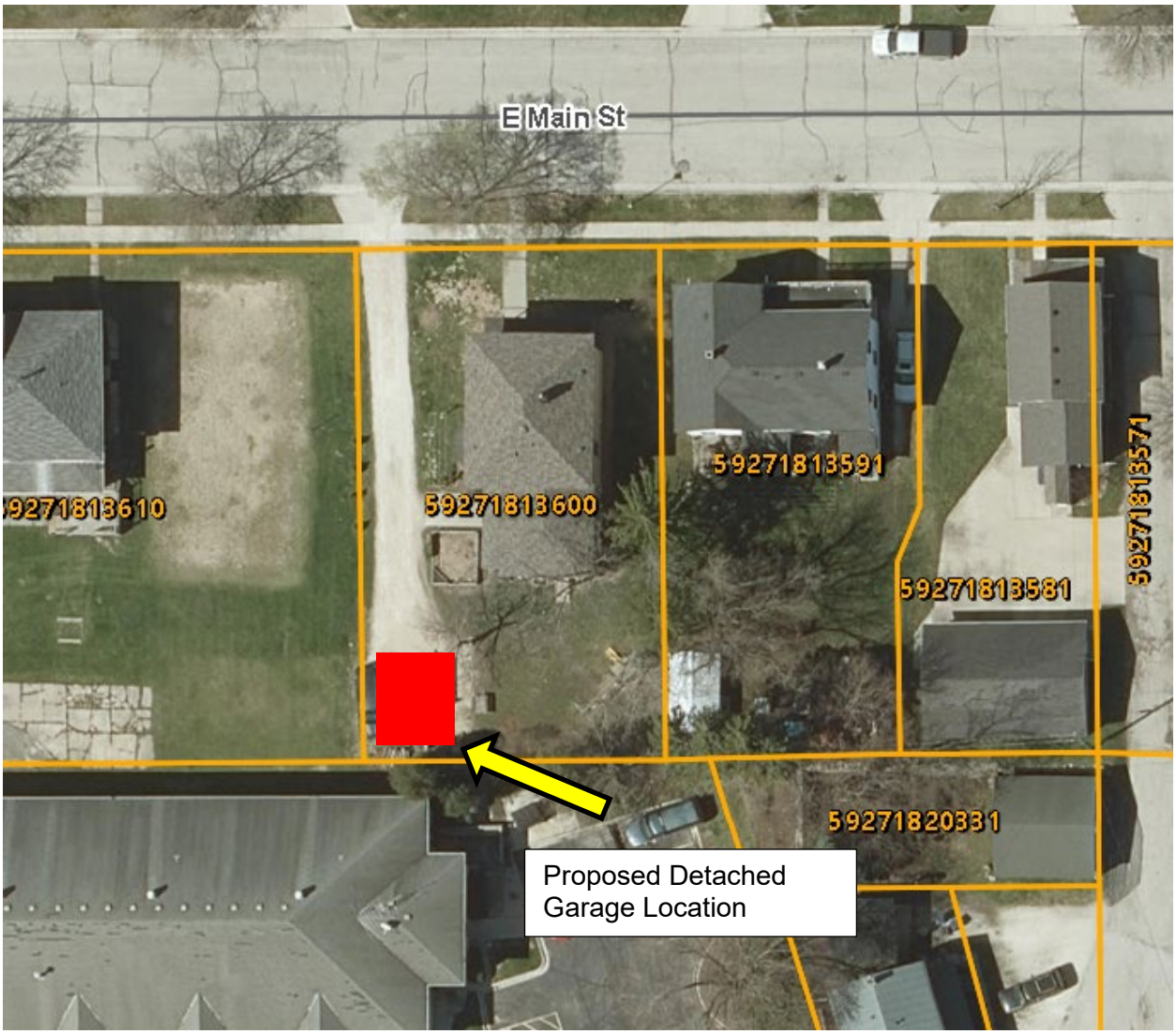
a. The appropriateness of the site plan and buildings in relation to the physical character of the site and the usage of adjoining land areas.
b. The layout of the site with regard to entrances and exits to public streets; the arrangement and improvement of interior roadways; the location, adequacy and improvement of areas for parking and for loading and unloading; and shall, in this connection, satisfy itself that the traffic pattern generated by the proposed construction or use shall be developed in a manner consistent with the safety of residents and the community, and the applicant shall so design the construction or use as to minimize any traffic hazard created thereby.
c. The adequacy of the proposed water supply, drainage facilities and sanitation and waste disposal.
d. The landscape and appearance of the completed site. The plan commission may require that those portions of all front, rear and side yards not used for off-street parking shall be attractively planted with trees, shrubs, plants or grass lawns, and that the site be effectively screened so as not to impair the value of adjacent properties nor impair the intent of purposes of this section.

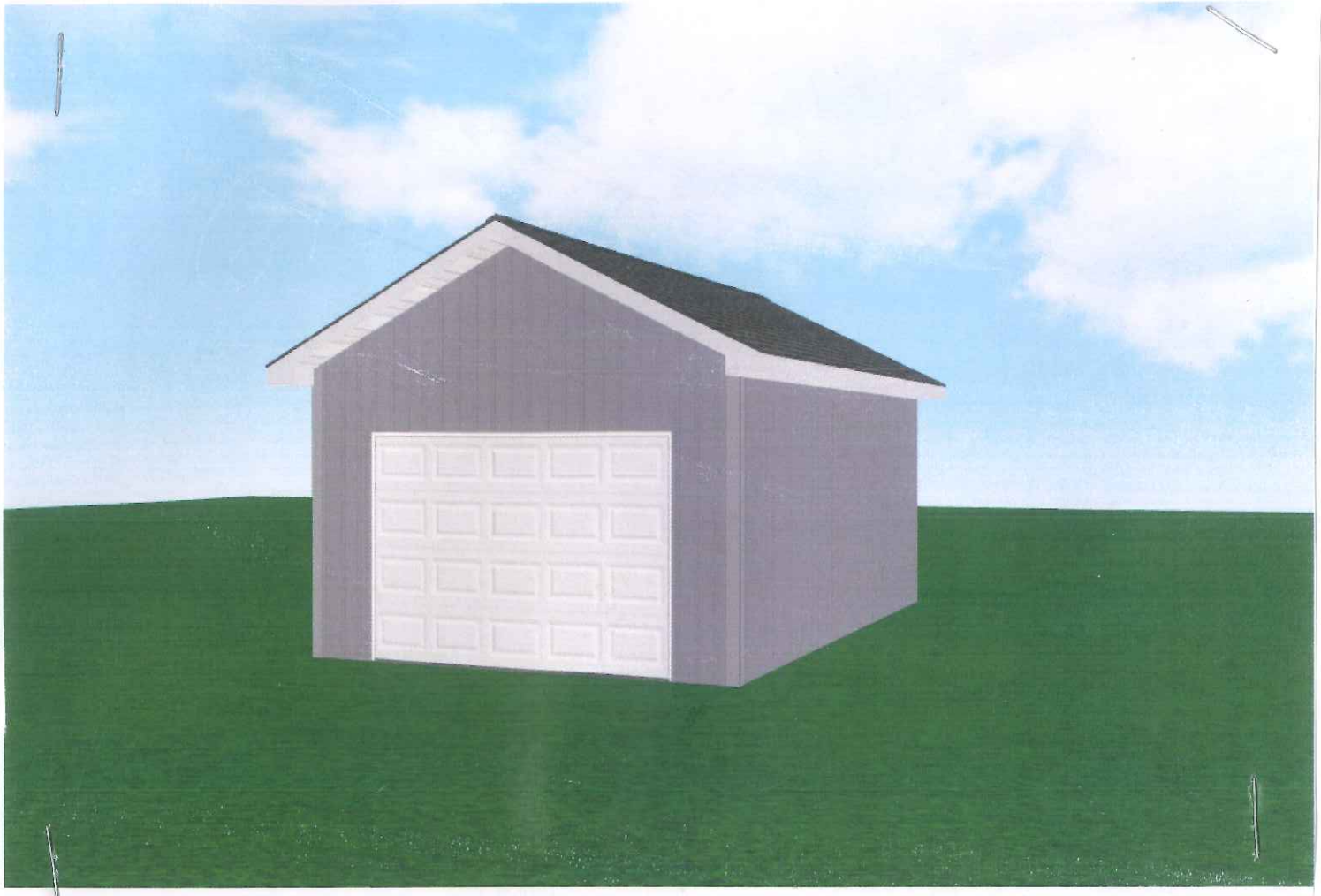
Staff Recommendation:
Staff recommends the Plan Commission approve the detached garage for the 615 East Main Street property.

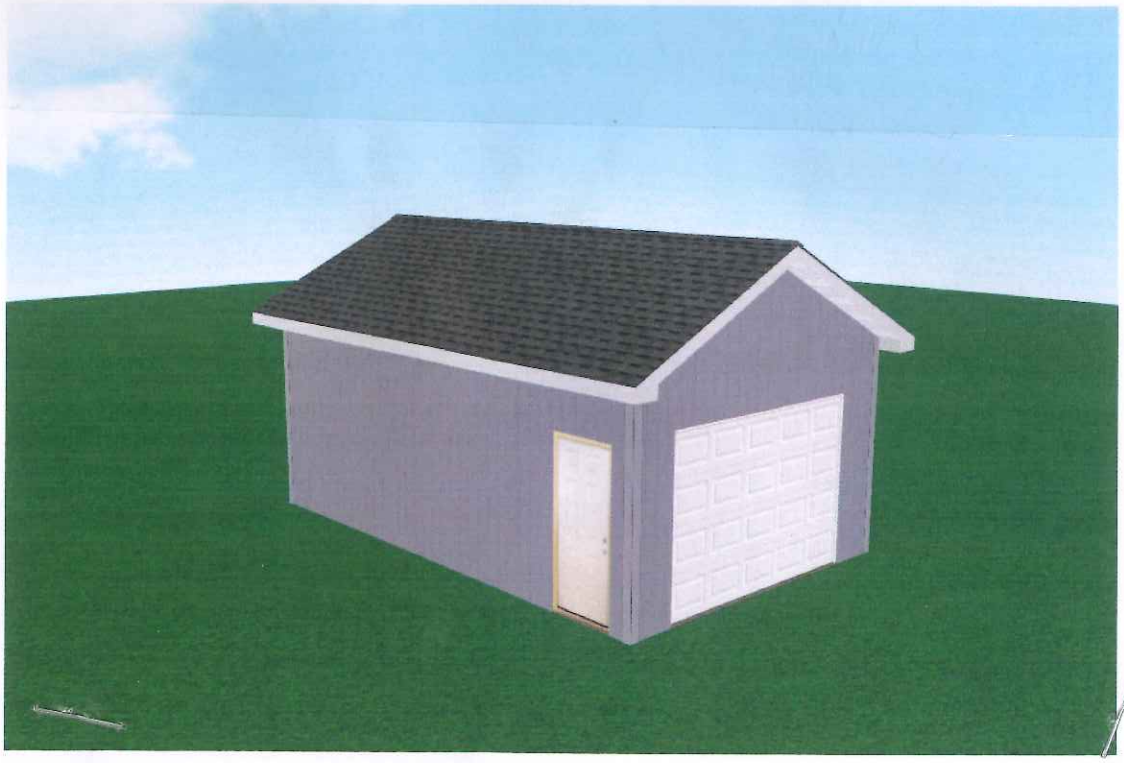
Copies Mailed/Emailed To:
I. Jon Puetz: jpthebuilder@live.com

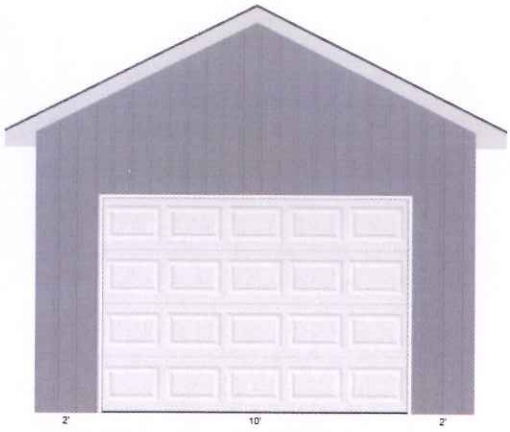
Attachments:
I. GIS Map
II. Building Plan Materials

Attachment I



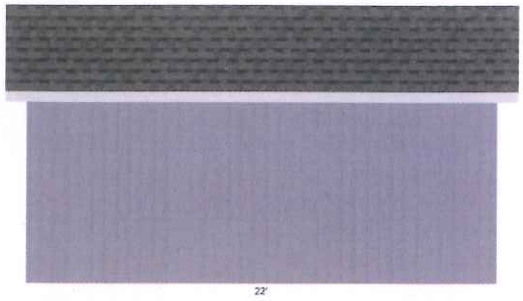






ENDWALL B

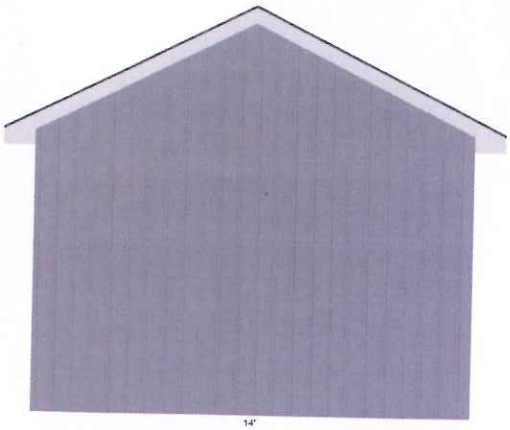
10x7 White Raised Panel Plain Torsion Spring (R-Value 6.5)



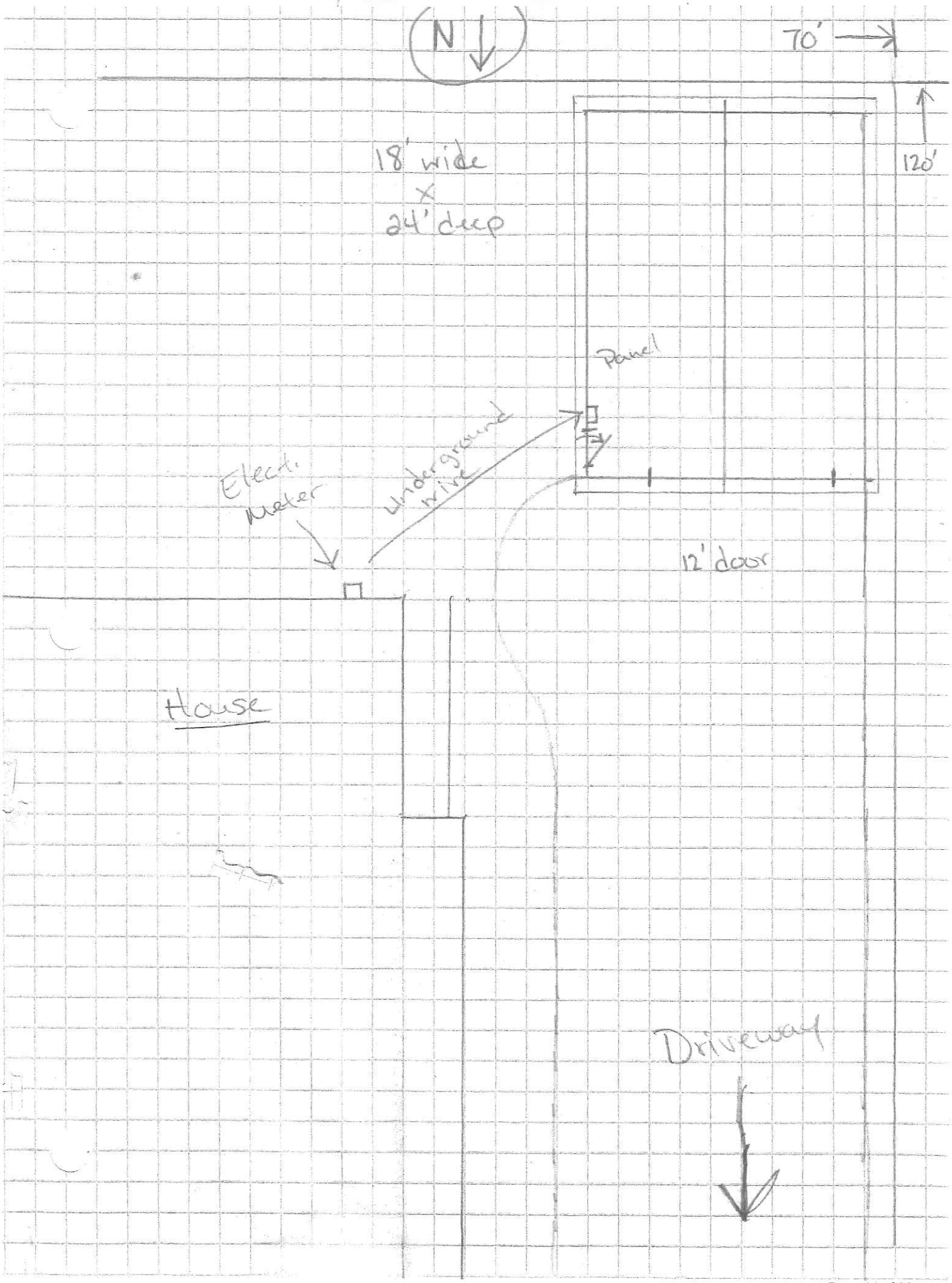
SIDEWALL D

SIDEWALL C

Mastercraft® 36W x 80H Primed Steel 6-Panel



ENDWALL A


N
70'
120'
18' wide
x
24' deep
Panel
Underground wire
Elect. Meter
12' door
House
Driveway

City of Plymouth
128 Smith St. - P.O. Box 107
Plymouth, WI 53073-0107

Telephone: **(920) 893-3745**
Facsimile: **(920) 893-0183**
Web Site: **plymouthgov.com**

DATE: August 24, 2025

TO: Plan Commission

FROM: Jack Johnston, Assistant City Administrator/Community Development Director

RE: **Site Plan Seeking Approval;** 3950 CTH PP, JJ Coolers LLC, for additional parking lot space and installation of a gravel trailer parking area. Keller, Inc. (enclosure)

Background:
Doug Schwalbe, Keller Inc., on behalf of JJ Coolers, LLC located at 3950 CTH PP in the City of Plymouth has submitted a site plan review application to increase employee and semi-trailer parking at their warehouse facility on the City's far east side. Parking lot alterations such as this require site plan review and approval by the Plan Commission per Sec. 13-1-173 of the Plymouth Municipal Code.

Longstanding Plan Commissioners may remember JJ Coolers seeking and receiving site plan approval for a warehouse expansion in late 2019 that included much of the same as what they are proposing now. That building expansion portion of the project has been put on hold, but they would like to move forward with the increase in parking. The other key change from the 2019 site plan is in regards to the semi-trailer parking. The 2019 plan showed heavy-duty asphalt while the current plan shows gravel parking in this location.

Comprehensive Plan/Zoning:
The property has H-I heavy industrial zoning which allows warehousing as a permitted use. The property is also designated to remain industrial per the 2022 Comprehensive Plan. As such, the current use and proposed addition are conforming to the City's zoning district for the property and Comprehensive Plan.

Parking Additions
In total, JJ Coolers would like to add ten (10) surface parking spaces for employee parking near the office portion of the building. These would be paved with concrete. Additionally, they would like to add 30 spots for semi-trailer parking. These would be gravel parking areas. Typically, gravel parking lots are not allowed by code. However, Section 13-1-92(c) notes that "the heavy industrial (HI) district is allowed to have gravel trailer parking or storage areas with approval by the plan commission." As JJ Coolers has the correct zoning and is planning to use the gravel parking lot for semi-trailer parking, staff recommends the Plan Commission approve the gravel parking lot as part of the overall site plan review and approval.

Code also requires adequate screening from these gravel lots "to avoid such gravel areas as being a dominant visual aspect of the site." The proposed gravel area is in the back of the property and is screened from the neighbor (Sartori Cheese) to the west with adequate plantings. Staff believes the existing plantings and property layout adhere to the ordinance.

Stormwater Management:
The applicant provided an updated stormwater management plan from 2019 for the previously approved site plan noted that the proposed gravel lot expansion falls within the footprint of previously proposed Phase 2 of development. Drainage patterns will remain the same with stormwater draining to the existing retention pond in the northwest corner of the property. Director of Public Works Cathy Austin has reviewed the document and confirmed conformance with City code on this item.

Staff Recommendation:
Staff recommends the Plan Commission approve the site plan changes for JJ Coolers and grant permission for the gravel parking area as allowed by Sec. 13-1-92(c) of the Plymouth Municipal Code.

Copies Mailed/Emailed To:
I. Doug Schwalbe: dschwalbe@kellerbuilds.com

Attachments:
I. Applicant Materials

August 7, 2025

Plymouth Plan Commission
Re: JJ Coolers – Site Work

Project Narrative:

This project includes two major goals. Provide additional car parking (10 paved spots) near office and provide additional semi truck/trailer parking (approx. 30 gravel spots) to support business growth. Also included is a concrete dolly pad as shown on civil drawings.

Most of this work has already been approved by the Plymouth Plan Commission in 2019. At that time a cooler building addition was also part of the project. None of that project went ahead due to business climate and a health issue that arose in JJ Cooler owner, Brian Lauersdorf.

The site work included takes the eventual addition of cooler space and loading dock positions into account for the future. Underground utilities will be installed that will allow for good drainable grades, the addition of dock positions, and the installation of future blacktop.

No changes will be made to the building itself.

Respectfully submitted:

KELLER, INC.
PLANNERS/ARCHITECTS/BUILDERS

Douglas Schwalbe, Project Manager, Keller Inc.

PROPOSED BUILDING ADDITION FOR:
J.J. COOLERS, LLC.
PLYMOUTH, WISCONSIN

LEGEND

- 000.00 — PROPOSED SPOT ELEVATIONS (FLOW LINE OF CURB UNLESS OTHERWISE SPECIFIED)
- 000.00 EG — EXISTING GRADE SPOT ELEVATIONS
- 000.00 FG — FUTURE GRADE SPOT ELEVATIONS
- 000.00 TR / 000.00 BR — PROPOSED SPOT ELEVATIONS (TOP OF RETAINING WALL, TOP OF SURFACE GRADE AT BOTTOM OF WALL)
- 000.00 TC / 000.00 BC — PROPOSED SPOT ELEVATIONS (TOP OF CURB, BOTTOM OF CURB)
- 000.00 TW / 000.00 BW — PROPOSED SPOT ELEVATIONS (TOP OF WALK, BOTTOM OF WALK)
- EXISTING WATER VALVE IN BOX
- PROPOSED WATER VALVE IN BOX
- EXISTING WATER VALVE IN MANHOLE
- EXISTING WATER SERVICE VALVE
- EXISTING TELEPHONE MANHOLE
- EXISTING ROUND CATCH BASIN
- PROPOSED ROUND CATCH BASIN
- EXISTING SQUARE CATCH BASIN
- EXISTING CURB INLET
- PROPOSED CURB INLET
- EXISTING UTILITY POLE
- EXISTING UTILITY POLE WITH GUY WIRE
- EXISTING STREET LIGHT
- EXISTING TELEPHONE PEDESTAL
- EXISTING ELECTRIC PEDESTAL
- EXISTING ELECTRIC BOX
- EXISTING CABLE TV PEDESTAL
- PROPOSED DRAINAGE FLOW
- 1-1/4" REBAR SET WEIGHING 4.30 LB/FT.
- 3/4" REBAR SET WEIGHING 1.50 LB/FT.
- 1-1/4" REBAR FOUND
- 3/4" REBAR FOUND
- 2" IRON PIPE FOUND
- 1" IRON PIPE FOUND
- EXISTING FLOOD LIGHT
- SECTION CORNER
- PROPOSED APRON ENDWALL
- EXISTING MARSH AREA
- EXISTING DECIDUOUS TREE WITH TRUNK DIAMETER
- EXISTING CONIFEROUS TREE
- EXISTING SHRUB
- EXISTING STUMP
- SOIL BORING
- EXISTING WELL
- PROPOSED WELL
- EXISTING LIGHT POLE
- EXISTING SIGN
- CENTER LINE
- EXISTING HANDICAP PARKING STALL
- PROPOSED HANDICAP PARKING STALL
- EXISTING GAS VALVE
- EXISTING WOODED AREA
- EXISTING HEDGE
- EXISTING CHAINLINK FENCE
- EXISTING WOOD FENCE
- EXISTING BARBED WIRE FENCE
- PROPERTY LINE
- EXISTING GUARD RAIL
- ST — EXISTING STORM SEWER AND MANHOLE
- ST — PROPOSED STORM SEWER AND MANHOLE
- SA — EXISTING SANITARY SEWER AND MANHOLE
- SA — PROPOSED SANITARY SEWER AND MANHOLE
- W — EXISTING WATER LINE AND HYDRANT
- W — PROPOSED WATER LINE AND HYDRANT
- OU — EXISTING OVERHEAD UTILITY LINE
- FO — EXISTING UNDERGROUND FIBER OPTIC LINE
- E — EXISTING UNDERGROUND ELECTRIC CABLE
- T — EXISTING UNDERGROUND TELEPHONE CABLE
- G — EXISTING UNDERGROUND GAS LINE
- PROPOSED CURB AND GUTTER
- EXISTING CURB AND GUTTER
- GRADING/SEEDING LIMITS
- RIGHT-OF-WAY LINE
- PROPERTY LINE
- RAILROAD TRACKS
- 800 — EXISTING GROUND CONTOUR
- 800 — PROPOSED GROUND CONTOUR

CIVIL SHEET INDEX

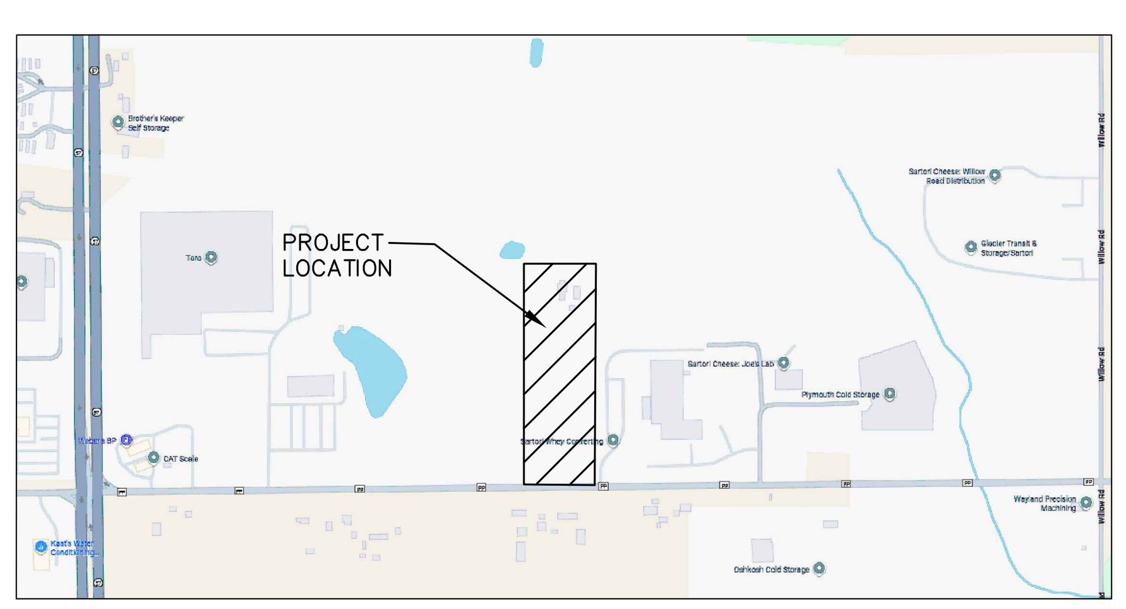
SHEET	SHEET TITLE
C1.0	CIVIL COVER AND SPECIFICATION SHEET
C1.1A	EXISTING SITE AND DEMOLITION PLAN
C1.1B	EXISTING SITE AND DEMOLITION PLAN
C1.2A	SITE PLAN
C1.2B	SITE PLAN
C1.3A	GRADING, EROSION CONTROL, AND UTILITY PLAN
C1.3B	GRADING, EROSION CONTROL, AND UTILITY PLAN
C2.0	DETAILS

TO OBTAIN LOCATION OF PARTICIPANTS' UNDERGROUND FACILITIES BEFORE YOU DIG IN WISCONSIN

CALL DIGGERS HOTLINE
1-800-242-8511

TOLL FREE
TELEFAX (414) 259-0947
TDD (FOR THE HEARING IMPAIRED) 1-800 542-2289

WISCONSIN STATUTE 182.0175 (1974)
REQUIRES MINIMUM OF 3 WORK DAYS
NOTICE BEFORE YOU EXCAVATE



PROJECT LOCATION MAP

PLAN SPECIFICATIONS

DIVISION 31 EARTH WORK

31 10 00 SITE CLEARING (DEMOLITION)

A. CONTRACTOR SHALL CALL DIGGER'S HOT LINE AND CONDUCT A PRIVATE UTILITY LOCATE AS REQUIRED TO ENSURE THAT ALL UTILITIES HAVE BEEN LOCATED BEFORE STARTING SITE DEMOLITION. DESIGN ENGINEER SHALL BE NOTIFIED OF ANY DISCREPANCIES BETWEEN PLAN AND FIELD CONDITIONS PRIOR TO CONSTRUCTION.
B. DEMOLITION PLAN IS AN OVERVIEW OF DEMOLITION TO TAKE PLACE ON SITE. CONTRACTOR TO FIELD VERIFY EXISTING SITE CONDITIONS PRIOR TO BIDDING. CONTRACTOR SHALL REMOVE, REPLACE, OR DEMOLISH ALL ITEMS AS NEEDED DURING CONSTRUCTION.
C. CONTRACTOR TO PROTECT EXISTING IMPROVEMENTS THAT ARE SCHEDULED TO REMAIN. ANY DAMAGE TO EXISTING FACILITIES SHALL BE REPLACED AT CONTRACTORS EXPENSE.
D. ALL CONCRETE NOTED TO BE REMOVED SHALL BE REMOVED TO THE NEAREST CONTROL JOINT.

31 20 00 EARTH MOVING

A. CONTRACTOR SHALL CALL DIGGER'S HOT LINE AND CONDUCT A PRIVATE UTILITY LOCATE AS REQUIRED TO ENSURE THAT ALL UTILITIES HAVE BEEN LOCATED BEFORE STARTING EXCAVATION. DESIGN ENGINEER SHALL BE NOTIFIED OF ANY DISCREPANCIES BETWEEN PLAN AND FIELD CONDITIONS PRIOR TO CONSTRUCTION.
B. PROVIDE ALL LABOR, MATERIALS AND EQUIPMENT FOR ALL EXCAVATION, GRADING, FILL AND BACKFILL WORK AS REQUIRED TO COMPLETE THE GENERAL CONSTRUCTION WORK. ALL EXCAVATION AND BACKFILL FOR ELECTRICALS AND MECHANICALS ARE THE RESPONSIBILITY OF THE RESPECTIVE CONTRACTOR.
C. ALL ORGANIC TOPSOIL INSIDE THE BUILDING AREA, UNDER PAVED AREAS, AND AT SITE FILL AREAS SHALL BE REMOVED. PROOF ROLL SUBGRADES BEFORE PLACING FILL WITH HEAVY PNEUMATIC-TIRED EQUIPMENT, SUCH AS A FULLY-LOADED TANDEM AXLE DUMP TRUCK, TO IDENTIFY SOFT POCKETS AND AREAS OF EXCESS YIELDING. CONTRACTOR SHALL VERIFY TOPSOIL DEPTHS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REVIEW AND FOLLOW THE RECOMMENDATIONS OF THE GEOTECHNICAL REPORT AND ACCOUNT FOR EXISTING CONDITIONS PRIOR TO SUBMITTING BID FOR THE PROJECT. EXCESS MATERIALS SHALL BE REMOVED FROM THE SITE UNLESS OTHERWISE DIRECTED IN THE PLANS OR BY LOCAL ZONING REQUIREMENTS.
D. PLACE AND COMPACT FILL MATERIAL IN LAYERS TO REQUIRED ELEVATIONS. UNIFORMLY MOISTEN OR AERATE SUBGRADE AND EACH SUBSEQUENT FILL OR BACKFILL LAYER BEFORE COMPACTION AS RECOMMENDED TO ACHIEVE SPECIFIED DRY DENSITY. REMOVE AND REPLACE, OR SCARIFY AND AIR DRY, OTHERWISE SATISFACTORY SOIL MATERIAL THAT IS TOO WET TO COMPACT TO SPECIFIED DRY DENSITY.
E. PLACE BACKFILL AND FILL MATERIALS IN LAYERS NOT MORE THAN 8" IN LOOSE DEPTH FOR MATERIAL COMPACTED BY HEAVY COMPACTION EQUIPMENT, AND NOT MORE THAN 4" IN LOOSE DEPTH FOR MATERIAL COMPACTED BY HAND-OPERATED TAMPERS.
F. COMPACT THE SOIL TO NOT LESS THAN THE FOLLOWING PERCENTAGES OF MAXIMUM DRY DENSITY ACCORDING TO ASTM D 698, STANDARD PROCTOR TEST. FILL MAY NOT BE PLACED ON FROZEN GROUND AND NO FROZEN MATERIALS MAY BE USED FOR BACK FILL. APPLY THE MORE STRINGENT REQUIREMENTS WHEN COMPARING BETWEEN THE FOLLOWING AND THE GEOTECHNICAL REPORT.
1. UNDER FOUNDATIONS - SUBGRADE, AND EACH LAYER OF BACKFILL OR FILL MATERIAL, TO NOT LESS THAN 98 PERCENT.
2. UNDER INTERIOR SLAB-ON-GRADE WHERE GROUNDWATER IS MORE THAN 3 FEET BELOW THE SLAB - PLACE A DRAINAGE COURSE LAYER OF 3/4" CRUSHED STONE, WITH 5% TO 12% FINES, PER THICKNESS INDICATED ON FOUNDATION PLANS ON PREPARED SUBGRADE. COMPACT THE SUBGRADE AND DRAINAGE COURSE TO NOT LESS THAN 95 PERCENT.
3. UNDER INTERIOR SLAB-ON-GRADE WHERE GROUNDWATER IS WITHIN 3 FEET OF THE SLAB SURFACE- PLACE A DRAINAGE COURSE LAYER OF CLEAN 3/4" CRUSHED STONE, WITH NO MORE THAN 5% FINES, PER THICKNESS INDICATED ON FOUNDATION PLANS ON PREPARED SUBGRADE. COMPACT THE SUBGRADE AND DRAINAGE COURSE TO NOT LESS THAN 95 PERCENT.
4. UNDER EXTERIOR CONCRETE AND ASPHALT PAVEMENTS - COMPACT THE SUBGRADE AND EACH LAYER OF BACKFILL OR FILL MATERIAL TO NOT LESS THAN 95 PERCENT.
5. UNDER WALKWAYS - COMPACT SUBGRADE AND EACH LAYER OF BACKFILL OR FILL MATERIAL TO NOT LESS THAN 95 PERCENT.
6. UNDER LAWN OR UNPAVED AREAS - COMPACT SUBGRADE AND EACH LAYER OF BACKFILL OR FILL MATERIAL, TO NOT LESS THAN 85 PERCENT.

G. CONTRACTOR SHALL ENGAGE A QUALIFIED INDEPENDENT TESTING AND INSPECTING AGENCY TO PERFORM FIELD TESTS AND INSPECTIONS. IT IS SUGGESTED THAT THE GEOTECHNICAL FIRM USED TO PERFORM THE SUBSURFACE SOIL INVESTIGATION BE ENGAGED FOR THE FIELD QUALITY CONTROL TESTS. THE GEOTECHNICAL REPORT WAS PERFORMED BY (NAME OF GEOTECHNICAL FIRM).
H. ALLOW THE TESTING AGENCY TO TEST AND INSPECT SUBGRADES AND EACH FILL OR BACKFILL LAYER. PROCEED WITH SUBSEQUENT EARTHWORK ONLY AFTER TEST RESULTS FOR PREVIOUSLY COMPLETED WORK COMPLY WITH REQUIREMENTS. PROVIDE ONE TEST FOR EVERY 2000 SQUARE FEET OF PAVED AREA OR BUILDING SLAB, ONE TEST FOR EACH SPREAD FOOTING, AND ONE TEST FOR EVERY 50 LINEAR FEET OF WALL STRIP FOOTING.
I. WHEN THE TESTING AGENCY REPORTS THAT SUBGRADES, FILLS, OR BACKFILLS HAVE NOT ACHIEVED DEGREE OF COMPACTION SPECIFIED, SCARIFY AND MOISTEN OR AERATE, OR REMOVE AND REPLACE SOIL TO DEPTH REQUIRED; RECOMPACT AND RETEST UNTIL SPECIFIED COMPACTION IS OBTAINED.
J. THE BUILDING SITE SHALL BE GRADED TO PROVIDE DRAINAGE AWAY FROM THE BUILDING AS INDICATED ON THE PLANS. SITE EARTHWORK SHALL BE GRADED TO WITHIN 0.10' OF REQUIRED EARTHWORK ELEVATIONS ASSUMING POSITIVE DRAINAGE IS MAINTAINED IN ACCORDANCE WITH THE GRADING PLAN.

A.

31 30 00 EROSION CONTROL/STORMWATER MANAGEMENT

A. THE DESIGN ENGINEER SHALL PREPARE A SITE SPECIFIC EROSION CONTROL AND A STORMWATER MANAGEMENT PLAN PURSUANT TO NR 216.46 AND NR 216.47. THE DESIGN ENGINEER SHALL ALSO FILE A CONSTRUCTION NOTICE OF INTENT WITH THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES PURSUANT TO NR 216.43 OR TO AN AUTHORIZED LOCAL PROGRAM PURSUANT TO NR 216.415 TO OBTAIN COVERAGE UNDER THE GENERAL WPDES STORM WATER PERMIT.
B. THE CONTRACTOR SHALL KEEP THE NOTICE OF INTENT PERMIT, APPROVED EROSION CONTROL AND STORMWATER MANAGEMENT PLANS, AND PLAN AMENDMENTS ON THE CONSTRUCTION SITE AT ALL TIMES UNTIL PERMIT COVERAGE IS TERMINATED.
C. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL LOCAL EROSION CONTROL PERMITS.
D. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEETING THE MONITORING, MAINTENANCE, AND REPORTING REQUIREMENTS OF NR 216.48. INSPECTIONS OF IMPLEMENTED EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES MUST AT A MINIMUM BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS AFTER A PRECIPITATION EVENT OF 0.5" OR MORE. A PRECIPITATION EVENT MAY BE CONSIDERED TO BE THE TOTAL AMOUNT OF PRECIPITATION RECORDED IN ANY CONTINUOUS 24-HOUR PERIOD. THE CONTRACTOR SHALL REPAIR OR REPLACE EROSION AND SEDIMENT CONTROL AS NECESSARY WITHIN 24 HOURS OF AN INSPECTION OR AFTER A DEPARTMENT NOTIFICATION WHERE REPAIR OR REPLACEMENT IS REQUESTED.
E. THE CONTRACTOR SHALL MAINTAIN, AT THE CONSTRUCTION SITE, WEEKLY WRITTEN REPORTS OF ALL INSPECTIONS CONDUCTED. WISCONSIN DNR CONSTRUCTION SITE INSPECTION REPORT FORM 3400-187 SHALL BE USED. WEEKLY INSPECTION REPORTS SHALL INCLUDE ALL OF THE FOLLOWING:
1. THE DATE, TIME, AND EXACT LOCATION OF THE CONSTRUCTION SITE INSPECTION.
2. THE NAME OF THE INDIVIDUAL WHO PERFORMED THE INSPECTION.
3. AN ASSESSMENT OF THE CONDITION OF THE EROSION AND SEDIMENT CONTROLS.
4. A DESCRIPTION OF ANY EROSION AND SEDIMENT CONTROL IMPLEMENTATION AND MAINTENANCE PERFORMED.
5. A DESCRIPTION OF THE PRESENT PHASE OF LAND DISTURBING CONSTRUCTION ACTIVITY AT THE CONSTRUCTION SITE.

F. EROSION AND SEDIMENT CONTROL IMPLEMENTED DURING CONSTRUCTION SHALL STRICTLY COMPLY WITH THE GUIDELINES AND REQUIREMENTS SET FORTH IN WISCONSIN ADMINISTRATIVE CODE (W.A.C.) NR 151, THE STATE OF WISCONSIN DEPARTMENT OF NATURAL RESOURCES RUNOFF MANAGEMENT PERFORMANCE STANDARDS. TECHNICAL STANDARDS PUBLISHED BY THE WISCONSIN DNR SHALL ALSO BE UTILIZED TO IMPLEMENT THE REQUIRED PERFORMANCE STANDARDS. THE METHODS AND TYPES OF EROSION CONTROL WILL BE DEPENDENT ON THE LOCATION AND TYPE OF WORK INVOLVED. ALL SEDIMENT CONTROL MEASURES SHALL BE ADJUSTED TO MEET FIELD CONDITIONS AT THE TIME OF CONSTRUCTION, AND INSTALLED PRIOR TO ANY GRADING OR DISTURBANCE OF EXISTING SURFACE MATERIAL. BELOW IS A LIST OF EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES TO ACHIEVE THE PERFORMANCE STANDARDS REQUIRED.
1. SILT FENCE SHALL BE PLACED ON SITE AT LOCATIONS SHOWN ON THE EROSION CONTROL PLAN. SILT FENCE SHALL ALSO BE PROVIDED AROUND THE PERIMETER OF ALL SOIL STOCKPILES. FOLLOW PROCEDURES FOUND IN WISCONSIN DNR TECHNICAL STANDARD 1056.
2. DITCH CHECKS SHALL BE PROVIDED TO REDUCE THE VELOCITY OF WATER FLOWING IN DITCH BOTTOMS. PLACE AT LOCATIONS SHOWN ON THE EROSION CONTROL PLAN. FOLLOW PROCEDURES FOUND IN WISCONSIN DNR TECHNICAL STANDARD 1062
3. STONE TRACKING PADS SHALL BE PLACED AT ALL CONSTRUCTION SITE ENTRANCES AND SHALL BE INSTALLED PRIOR TO ANY TRAFFIC LEAVING THE CONSTRUCTION SITE. SEE THE EROSION CONTROL PLAN FOR LOCATIONS. THE AGGREGATE USED SHALL BE 3 TO 6 INCH CLEAR OR WASHED STONE, AND SHALL BE PLACED IN A LAYER AT LEAST 12 INCHES THICK. THE STONE SHALL BE UNDERLAIN WITH A WISDOT TYPE R GEOTEXTILE FABRIC. THE TRACKING PAD SHALL BE THE FULL WIDTH OF THE EGRESS POINT, AND SHALL BE A MINIMUM OF 50 FEET LONG. SURFACE WATER MUST BE PREVENTED FROM PASSING THROUGH THE TRACKING PAD. FOLLOW PROCEDURES FOUND IN WISCONSIN DNR TECHNICAL STANDARD 1057.
4. STORM DRAIN INLET PROTECTION SHALL BE PROVIDED FOR ALL NEW AND DOWNSTREAM STORM CATCH BASINS AND CURB INLETS. TYPE B OR C PROTECTION SHOULD BE PROVIDED AND SHALL BE IN CONFORMANCE WITH WISCONSIN DNR TECHNICAL STANDARD 1060.
5. DUST CONTROL MEASURES SHALL BE PROVIDED TO REDUCE OR PREVENT THE SURFACE AND AIR TRANSPORT OF DUST DURING CONSTRUCTION. CONTROL MEASURES INCLUDE APPLYING MULCH AND ESTABLISHING VEGETATION, WATER SPRAYING, SURFACE ROUGHENING, APPLYING POLYMERS, SPRAY-ON TACKIFIERS, CHLORIDES, AND BARRIERS. SOME SITES MAY REQUIRE AN APPROACH THAT UTILIZES A COMBINATION OF MEASURES FOR DUST CONTROL. FOLLOW PROCEDURES FOUND IN WISCONSIN DNR TECHNICAL STANDARD 1068.
6. THE USE, STORAGE, AND DISPOSAL OF CHEMICALS, CEMENT, AND OTHER COMPOUNDS AND MATERIALS USED ON SITE SHALL BE MANAGED DURING THE CONSTRUCTION PERIOD TO PREVENT THEIR TRANSPORT BY RUNOFF INTO WATERS OF THE STATE.
7. CONTRACTOR SHALL PROVIDE AN OPEN AGGREGATE CONCRETE TRUCK WASHOUT AREA ON SITE. CONTRACTOR TO ENSURE THAT CONCRETE WASHOUT SHALL BE CONTAINED TO THIS DESIGNATED AREA AND NOT BE ALLOWED TO RUN INTO STORM INLETS OR INTO THE OVERLAND STORMWATER DRAINAGE SYSTEM. WASHOUT AREA SHALL BE REMOVED UPON COMPLETION OF CONSTRUCTION.
8. TEMPORARY SITE RESTORATION SHALL TAKE PLACE IN DISTURBED AREAS THAT WILL NOT BE BROUGHT TO FINAL GRADE OR ON WHICH LAND DISTURBING ACTIVITIES WILL NOT BE PERFORMED FOR A PERIOD GREATER THAN 14 DAYS AND REQUIRES VEGETATIVE COVER FOR LESS THAN ONE YEAR. THIS TEMPORARY SITE RESTORATION REQUIREMENT ALSO APPLIES TO SOIL STOCKPILES. PERMANENT RESTORATION APPLIES TO AREAS WHERE PERENNIAL VEGETATIVE COVER IS NEEDED TO PERMANENTLY STABILIZE AREAS OF EXPOSED SOIL. PERMANENT STABILIZATION SHALL OCCUR WITHIN 3 WORKING DAYS OF FINAL GRADING. TOPSOIL, SEED, AND MULCH SHALL BE IN GENERAL CONFORMANCE WITH TECHNICAL STANDARDS 1058 AND 1059 AND SHALL MEET THE SPECIFICATIONS FOUND IN THE LANDSCAPING AND SITE STABILIZATION SECTION OF THIS CONSTRUCTION DOCUMENT. ANY SOIL EROSION THAT OCCURS AFTER FINAL GRADING AND/OR FINAL STABILIZATION MUST BE REPAIRED AND THE STABILIZATION WORK REDONE.
9. IF SITE DEWATERING IS REQUIRED TO REMOVE SEDIMENT FROM CONSTRUCTION SITE STORMWATER PRIOR TO DISCHARGING OFF-SITE OR TO WATERS OF THE STATE, FOLLOW PROCEDURES FOUND IN TECHNICAL STANDARD 1061.
10. ALL OFF-SITE SEDIMENT DEPOSITS OCCURRING AS A RESULT OF CONSTRUCTION WORK OR A STORM EVENT SHALL BE CLEANED UP BY THE END OF EACH WORKING DAY. FLUSHING SHALL NOT BE ALLOWED.

G. EROSION CONTROL MEASURES SHALL NOT BE REMOVED UNTIL THE AREA(S) SERVED HAVE ESTABLISHED VEGETATIVE COVER.
H. ONCE THE CONSTRUCTION SITE HAS BEEN FULLY STABILIZED AND TEMPORARY EROSION CONTROL BEST MANAGEMENT PRACTICES HAVE BEEN REMOVED, THE CONTRACTOR SHALL FILE A CONSTRUCTION NOTICE OF TERMINATION WITH THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES.
I. AT THE COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL GIVE THE OWNER COPIES OF THE EROSION CONTROL AND STORM WATER MANAGEMENT PLANS, AMENDMENTS TO PLANS, SUPPORTING PLAN DATA, AND CONSTRUCTION SITE EROSION CONTROL INSPECTION REPORTS. THE OWNER SHALL RETAIN THESE FOR A PERIOD OF 3 YEARS FROM THE DATE OF TERMINATING COVERAGE UNDER WPDES GENERAL PERMIT.
J. ALL POST CONSTRUCTION STORMWATER MANAGEMENT BEST MANAGEMENT PRACTICES SHALL BE CONSTRUCTED BEFORE THE SITE HAS UNDERGONE FINAL STABILIZATION.

DIVISION 32 EXTERIOR IMPROVEMENTS

32 10 00 GRAVEL AREAS

A. CONTRACTOR TO PROVIDE COMPACTED GRAVEL WHERE INDICATED ON THE PLANS. ALL AGGREGATE PROVIDED MUST COMPLY WITH SECTION 305 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION. CONTRACTOR TO PROVIDE AGGREGATE TYPES AND DEPTHS AS INDICATED BELOW:

6" OF 3/4" CRUSHED AGGREGATE
6" OF 3" CRUSHED AGGREGATE

B. CONTRACTOR TO COMPACT THE AGGREGATE TO AN AVERAGE DENSITY PER WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION. ALL GRAVEL AREAS SHALL BE GRADED TO WITHIN 0.10' OF DESIGN SURFACE GRADES WITH POSITIVE DRAINAGE BEING MAINTAINED IN ACCORDANCE WITH DESIGN PLANS. A MINIMUM OF 1% SLOPE SHALL BE MAINTAINED IN ALL GRAVEL AREAS.

32 20 00 AGGREGATE BASE & ASPHALT PAVEMENT

A. CONTRACTOR TO PROVIDE COMPACTED AGGREGATE BASE AND HOT MIX ASPHALT PAVEMENT WHERE INDICATED ON THE PLANS. ALL AGGREGATE PROVIDED MUST COMPLY WITH SECTION 305 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION. PROVIDE HOT MIX ASPHALT MIXTURE TYPES PER SECTION 460 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION. CONTRACTOR TO PROVIDE AGGREGATE BASE AND HOT MIX ASPHALT PAVEMENT TYPES AND DEPTHS AS INDICATED BELOW:

STANDARD ASPHALT PAVING
1-1/2" SURFACE COURSE (5 LT 58-28S)
1-1/2" BINDER COURSE (3 LT 58-28S)
4" OF 1-1/4" CRUSHED AGGREGATE
6" OF 3" CRUSHED AGGREGATE

B. CONTRACTOR TO COMPACT THE AGGREGATE BASE, ASPHALT BINDER COURSE, AND ASPHALT SURFACE COURSE TO AN AVERAGE DENSITY PER WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION. ALL ASPHALT PAVEMENT AREAS SHALL BE PAVED TO WITHIN 0.10' OF DESIGN SURFACE GRADES WITH POSITIVE DRAINAGE BEING MAINTAINED IN ACCORDANCE WITH DESIGN PLANS. A MINIMUM OF 1% SLOPE SHALL BE MAINTAINED IN ALL ASPHALT PAVEMENT AREA.
C. HOT MIX ASPHALT CONSTRUCTION TO BE PROVIDED PER MORE STRINGENT REQUIREMENTS OF GEOTECHNICAL REPORT OR CONSTRUCTION DOCUMENTS.
D. CONTRACTOR TO PROVIDE 4" WIDE RED PAINTED STRIPING FOR PARKING STALLS, TRAFFIC LANES, AND NO PARKING AREAS. (YELLOW OR WHITE) PAINT MARKINGS SHALL ALSO BE PROVIDED FOR H.C. ACCESSIBLE SYMBOLS, TRAFFIC ARROWS, AND TRAFFIC MESSAGES.

32 30 00 CONCRETE AND AGGREGATE BASE

A. CONTRACTOR TO PROVIDE CRUSHED AGGREGATE BASE AND CONCRETE WHERE INDICATED ON THE PLANS.
B. ALL AGGREGATE PROVIDED MUST COMPLY WITH SECTION 305 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION. ALL AGGREGATE PLACED MUST BE COMPACTED TO AN AVERAGE DENSITY PER WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION.
C. DESIGN AND CONSTRUCTION OF ALL CAST-IN-PLACE EXTERIOR CONCRETE FLAT WORK SHALL CONFORM TO ACI 330R-08.
D. EXTERIOR CONCRETE FLAT WORK CONSTRUCTION TO BE PROVIDED PER MORE STRINGENT REQUIREMENTS OF THE GEOTECHNICAL REPORT OR THIS SPECIFICATION. CONCRETE FLAT WORK CONSTRUCTION IS AS FOLLOWS:
1. DOLLEY PAD CONCRETE-8" OF CONCRETE OVER 6" OF ¾ CRUSHED AGGREGATE BASE. CONCRETE SHALL BE REINFORCED WITH 6 X 6 #6 W.W.M.

E. DESIGN MIXES SHALL BE IN ACCORDANCE WITH ASTM C94
1. STRENGTH TO BE MINIMUM OF 4,000 PSI AT 28 DAYS FOR EXTERIOR CONCRETE.
2. SLUMP SHALL NOT EXCEED 4" FOR EXTERIOR CONCRETE FLAT WORK
3. SLUMP SHALL BE 2.5" OR LESS FOR SLIP-FORMED CURB AND GUTTER
4. SLUMP SHALL BE BETWEEN 1.5" TO 3" FOR NON SLIP-FORMED CURB AND GUTTER.
5. ALL EXTERIOR CONCRETE SHALL BE AIR ENTRAINED WITH 4% TO 7% AIR CONTENT. NO OTHER ADMIXTURES SHALL BE USED WITHOUT APPROVAL OF EXCEL ENGINEERING, INC. CALCIUM CHLORIDE SHALL NOT BE USED.
6. MAXIMUM AGGREGATE SIZE FOR ALL EXTERIOR CONCRETE SHALL BE 0.75 INCHES.

F. VERIFY EQUIPMENT CONCRETE PAD SIZES WITH RESPECTIVE CONTRACTORS. PADS SHALL HAVE FIBERMESH 300 FIBERS AT A RATE OF 1.5 LBS/CU. YD. OR 6 X 6-W1.4 X W1.4 WELDED WIRE MESH WITH MINIMUM 1 INCH COVER. EQUIPMENT PADS SHALL BE 3.5 INCHES THICK WITH 1 INCH CHAMFER UNLESS SPECIFIED OTHERWISE. COORDINATE ADDITIONAL PAD REQUIREMENTS WITH RESPECTIVE CONTRACTOR.
G. ALL CONCRETE FLAT WORK SURFACES AND CONCRETE CURB FLOWLINES SHALL BE CONSTRUCTED TO WITHIN 0.05' OF DESIGN SURFACE AND FLOWLINE GRADES ASSUMING POSITIVE DRAINAGE IS MAINTAINED IN ACCORDANCE WITH THE DESIGN PLANS.
H. CONCRETE FLAT WORK SHALL HAVE CONSTRUCTION JOINTS OR SAW CUT JOINTS PLACED AS INDICATED ON THE PLANS OR PER THIS SPECIFICATION. SAWCUTS SHALL BE DONE AS SOON AS POSSIBLE, BUT NO LATER THAN 24 HOURS AFTER CONCRETE IS PLACED. CONCRETE CURB AND GUTTER JOINTING SHALL BE PLACED EVERY 10' OR CLOSER (6' MIN.). ALL EXTERIOR CONCRETE SHALL HAVE A LIGHT BROOM FINISH UNLESS NOTED OTHERWISE. A UNIFORM COAT OF A HIGH SOLIDS CURING COMPOUND MEETING ASTM C309 SHOULD BE APPLIED TO ALL EXPOSED CONCRETE SURFACES. ALL CONCRETE IS TO BE CURED FOR 7 DAYS. EXTERIOR CONCRETE SHALL BE SEPARATED FROM BUILDINGS WITH CONTINUOUS 0.5 INCH FIBER EXPANSION JOINT AND/OR 0.25 INCH FIBER EXPANSION JOINT AT DECORATIVE MASONRY UNITS.
I. ALL REINFORCING BARS SHALL BE ASTM A615 GRADE 60. THICKNESS OF CONCRETE COVER OVER REINFORCEMENT SHALL BE NOT LESS THAN 3" WHERE CONCRETE IS DEPOSITED AGAINST THE GROUND WITHOUT THE USE OF FORMS AND NOT LESS THAN 1.5" IN ALL OTHER LOCATIONS. ALL REINFORCING SHALL BE LAPPED 36 DIAMETERS FOR UP TO #6 BARS, 60 DIAMETERS FOR #7 TO #10 BARS OR AS NOTED ON THE DRAWINGS AND EXTENDED AROUND CORNERS WITH CORNER BARS. PLACING AND DETAILING OF STEEL REINFORCING AND REINFORCING SUPPORTS SHALL BE IN ACCORDANCE WITH CRSI AND ACI MANUAL AND STANDARD PRACTICES. THE REINFORCEMENT SHALL NOT BE PAINTED AND MUST BE FREE OF GREASE/OIL, DIRT OR DEEP RUST WHEN PLACED IN THE WORK. ALL WELDED WIRE FABRIC SHALL MEET THE REQUIREMENTS OF ASTM A 185. WELDED WIRE FABRIC SHALL BE PLACED 2" FROM TOP OF SLAB, UNLESS INDICATED OTHERWISE.
J. CONTRACTOR SHALL ENGAGE A QUALIFIED INDEPENDENT TESTING AND INSPECTING AGENCY TO SAMPLE MATERIALS, PERFORM TESTS, AND SUBMIT TEST REPORTS DURING CONCRETE PLACEMENT. TESTS WILL BE PERFORMED ACCORDING TO ACI 301. CAST AND LABORATORY CURE ONE SET OF FOUR STANDARD CYLINDERS FOR EACH COMPOSITE SAMPLE FOR EACH DAY'S POUR OF EACH CONCRETE MIX EXCEEDING 5 CU. YD., BUT LESS THAN 25 CU. YD., PLUS ONE SET FOR EACH ADDITIONAL 50 CU. YD. OR FRACTION THEREOF. PERFORM COMPRESSIVE-STRENGTH TESTS ACCORDING TO ASTM C 39. TEST TWO SPECIMENS AT 7 DAYS AND TWO SPECIMENS AT 28 DAYS. PERFORM SLUMP TESTING ACCORDING TO ASTM C 143. PROVIDE ONE TEST AT POINT OF PLACEMENT FOR EACH COMPOSITE SAMPLE, BUT NOT LESS THAN ONE TEST FOR EACH DAY'S POUR OF EACH CONCRETE MIX. PERFORM ADDITIONAL TESTS WHEN CONCRETE CONSISTENCY APPEARS TO CHANGE.
K. PROTECT FRESHLY PLACED CONCRETE FROM PREMATURE DRYING AND EXCESSIVE COLD OR HOT TEMPERATURES. IN HOT, DRY, AND WINDY WEATHER, APPLY AN EVAPORATION-CONTROL COMPOUND ACCORDING TO MANUFACTURER'S INSTRUCTIONS AFTER SCREEDING AND BULL FLOATING, BUT BEFORE POWER FLOATING AND TROWELLING.
L. LIMIT MAXIMUM WATER-CEMENTIOUS RATIO OF CONCRETE EXPOSED TO FREEZING, THAWING AND DEICING SALTS TO 0.45.
M. TEST RESULTS WILL BE REPORTED IN WRITING TO THE DESIGN ENGINEER, READY-MIX PRODUCER, AND CONTRACTOR WITHIN 24 HOURS AFTER TESTS. REPORTS OF COMPRESSIVE STRENGTH TESTS SHALL CONTAIN THE PROJECT IDENTIFICATION NAME AND NUMBER, DATE OF CONCRETE PLACEMENT, NAME OF CONCRETE TESTING SERVICE, CONCRETE TYPE AND CLASS, LOCATION OF CONCRETE BATCH IN STRUCTURE, DESIGN COMPRESSIVE STRENGTH AT 28 DAYS, CONCRETE MIX PROPORTIONS AND MATERIALS, COMPRESSIVE BREAKING STRENGTH, AND TYPE OF BREAK FOR BOTH 7-DAY TESTS AND 28-DAY TESTS.
N. CONTRACTOR TO PROVIDE 4" WIDE RED PAINTED STRIPING FOR PARKING STALLS, TRAFFIC LANES, AND NO PARKING AREAS. (YELLOW OR WHITE) PAINT MARKINGS SHALL ALSO BE PROVIDED FOR H.C. ACCESSIBLE SYMBOLS, TRAFFIC ARROWS, AND TRAFFIC MESSAGES.

32 30 00 LANDSCAPING AND SITE STABILIZATION

A. TOPSOIL: CONTRACTOR TO PROVIDE A MINIMUM OF 6" OF TOPSOIL FOR ALL DISTURBED OPEN AREAS. REUSE SURFACE SOIL STOCKPILED ON SITE AND SUPPLEMENT WITH IMPORTED OR MANUFACTURED TOPSOIL FROM OFF SITE SOURCES WHEN QUANTITIES ARE INSUFFICIENT. PROVIDE SOIL ANALYSIS BY A QUALIFIED SOIL TESTING LABORATORY AS REQUIRED TO VERIFY THE SUITABILITY OF SOIL TO BE USED AS TOPSOIL AND TO DETERMINE THE NECESSARY SOIL AMENDMENTS. TEST SOIL FOR PRESENCE OF ATRAZINE AND INFORM EXCEL ENGINEERING, INC. IF PRESENT PRIOR TO BIDDING PROJECT. TOPSOIL SHALL HAVE A PH RANGE OF 5.5 TO 8, CONTAIN A MINIMUM OF 5 PERCENT ORGANIC MATERIAL CONTENT, AND SHALL BE FREE OF STONES 1 INCH OR LARGER IN DIAMETER. ALL MATERIALS HARMFUL TO PLANT GROWTH SHALL ALSO BE REMOVED.
TOPSOIL INSTALLATION: LOOSEN SUBGRADE TO A MINIMUM DEPTH OF 6 INCHES AND REMOVE STONES LARGER THAN 1" IN DIAMETER. ALSO REMOVE ANY STICKS, ROOTS, RUBBISH, AND OTHER EXTRANEOUS MATTER AND DISPOSE OF THEM OFF THE PROPERTY. SPREAD TOPSOIL TO A DEPTH OF 6" BUT NOT LESS THAN WHAT IS REQUIRED TO MEET FINISHED GRADES AFTER LIGHT ROLLING AND NATURAL SETTLEMENT. DO NOT SPREAD TOPSOIL IF SUBGRADE IS FROZEN, MUDDY, OR EXCESSIVELY WET. GRADE PLANTING AREAS TO A SMOOTH, UNIFORM SURFACE PLANE WITH LOOSE, UNIFORMLY FINE TEXTURE. GRADE TO WITHIN 0.05 FEET OF FINISHED GRADE ELEVATION.
B. SEEDED LAWNS:
1. PERMANENT LAWN AREAS SHALL BE SEEDED WITH THE FOLLOWING MIXTURE: 65% KENTUCKY BLUEGRASS BLEND (2.0-2.6 LBS./1,000 S.F.), 20% PERENNIAL RYEGRASS (0.6-0.8 LBS./1,000 S.F.), 15% FINE FESCUE (0.4-0.6 LBS/1,000 S.F.). STRAW AND MULCH SHALL BE LAID AT 100LBS/1,000 S.F. FERTILIZE AS PER SOIL TEST OR APPLY 5-10-10 OR EQUIVALENT AT 5-6 LBS/1,000 S.F. SEE EROSION MATTING SPECIFICATIONS AS REQUIRED. ALL SITE DISTURBED AREAS NOT DESIGNATED FOR OTHER LANDSCAPING AND SITE STABILIZATION METHODS SHALL BE SEEDED AS PERMANENT LAWN. NO BARE TOPSOIL SHALL BE LEFT ONSITE.
2. ALL PERMANENT AND TEMPORARY STORM WATER CONVEYANCE SWALE BOTTOMS AND SIDE SLOPES AS WELL AS STORMWATER MANAGEMENT BASIN BOTTOMS AND SIDE SLOPES SHALL BE SEEDED WITH THE FOLLOWING MIXTURE: 45% KENTUCKY BLUEGRASS (0.60 LBS./1000 S.F.), 40% CREEPING RED FESCUE (0.50 LBS./1,000 S.F.), AND 15% PERENNIAL RYEGRASS (0.20 LBS./1,000 S.F.). FERTILIZE AS PER SOIL TEST OR APPLY 5-10-10 OR EQUIVALENT AT 5-6 LBS./1,000 S.F. SEE EROSION MATTING SPECIFICATIONS AS REQUIRED.
3. ALL TEMPORARY SEEDING SHALL CONSIST OF THE FOLLOWING MIXTURE: 100% RYEGRASS AT 1.9 LBS./1,000 S.F. STRAW AND MULCH SHALL BE LAID AT 100 LBS./1,000 S.F. FERTILIZE AS PER SOIL TEST OR APPLY 5-10-10 OR EQUIVALENT AT 5-6 LBS./1,000 S.F. SEE EROSION MATTING SPECIFICATIONS AS REQUIRED.

C. SEEDED LAWN MAINTENANCE: CONTRACTOR TO PROVIDE MAINTENANCE OF ALL LANDSCAPING FOR A PERIOD OF 90 DAYS FROM THE DATE OF INSTALLATION. AT THE END OF THE MAINTENANCE PERIOD, A HEALTHY, UNIFORM, CLOSE STAND OF GRASS SHOULD BE ESTABLISHED FREE OF WEEDS AND SURFACE IRREGULARITIES. LAWN COVERAGE SHOULD EXCEED 90% AND BARE SPOTS SHOULD NOT EXCEED 5"X5". CONTRACTOR SHOULD REESTABLISH LAWNS THAT DO NOT COMPLY WITH THESE REQUIREMENTS AND CONTINUE MAINTENANCE UNTIL LAWNS ARE SATISFACTORY.
D. EROSION MATTING:
1. CONTRACTOR TO PROVIDE EROSION CONTROL MATTING (NORTH AMERICAN GREEN S150) OR EQUIVALENT ON ALL SLOPES THAT ARE 4:1 AND GREATER OUTSIDE OF STORMWATER CONVEYANCE SWALES AND STORMWATER MANAGEMENT BASINS.
2. CONTRACTOR TO PROVIDE EROSION MATTING (NORTH AMERICAN GREEN C125) OR EQUIVALENT IN ALL SWALE BOTTOMS AND SIDE SLOPES AS WELL AS STORMWATER MANAGEMENT BASIN BOTTOMS AND SIDE SLOPES AS REQUIRED.

E. RIP RAP: ALL RIP RAP ASSOCIATED WITH STORMWATER MANAGEMENT AND STORMWATER CONVEYANCE, AS DELINEATED ON THE PLANS, SHALL BE CONSTRUCTED WITH THE TOP OF RIP RAP MATCHING THE PROPOSED ADJACENT GRADE ELEVATIONS. PLACEMENT OF RIP RAP ABOVE THE PROPOSED ADJACENT GRADE ELEVATIONS IS NOT ACCEPTABLE. ALL RIP RAP SHALL BE PLACED ON TYPE HR FILTER FABRIC PER SECTION 645 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURAL CONSTRUCTION.
F. MINERAL MULCH: PROVIDE 3" MINIMUM THICK BLANKET OF 0.75" MINIMUM TO 1.5" MAXIMUM CRUSHED DECORATIVE STONE AT ALL PLANTING AREAS INDICATED ON THE LANDSCAPE PLAN. INSTALL OVER NON-WOVEN WEED BARRIER FABRIC. COLOR BY OWNER.
G. PLASTIC EDGING: INSTALL VALLEY VIEW INDUSTRIES BLACK DIAMOND LAWN EDGING TO SEPARATE ALL PLANTING BEDS FROM LAWN AREAS. EDGING TO BE 5.5" TALL WITH METAL STAKES INSTALLED PER MANUFACTURER'S WRITTEN INSTRUCTIONS.

DIVISION 33 UTILITIES

33 10 00 SITE UTILITIES

A. CONTRACTOR TO FIELD VERIFY ALL EXISTING UNDERGROUND UTILITIES ON SITE. CONTRACTOR TO VERIFY PIPE LOCATIONS, SIZES, AND DEPTHS AT POINT OF PROPOSED CONNECTIONS AND VERIFY PROPOSED UTILITY ROUTES ARE CLEAR (PER CODE) OF ALL EXISTING UTILITIES AND OTHER OBSTRUCTIONS PRIOR TO CONSTRUCTION. COSTS INCURRED FOR FAILURE TO DO SO SHALL BE THE CONTRACTORS RESPONSIBILITY.
B. ALL PROPOSED WATER PIPE SHALL BE C906 PE FOR PIPE DIAMETERS OF 4" OR LESS, C900 PVC FOR PIPE DIAMETERS OF 6" THROUGH 12", AND C-905 PVC FOR PIPE DIAMETERS OF 14" THROUGH 36". 6' MINIMUM COVER SHALL BE PROVIDED OVER ALL WATER PIPING UNLESS OTHERWISE SPECIFIED.
C. ALL PROPOSED HDPE STORM PIPE SHALL BE IN ACCORDANCE WITH ASTM F2648. ALL CONCRETE STORM PIPING SHALL BE IN ACCORDANCE WITH ASTM C14 AND ASTM C76. ALL PVC PIPE TO BE SCH. 40. CULVERT TO BE CMP AS APPROVED BY COUNTY HIGHWAY DEPT. PIPE SHALL BE PLACED MIN. 8' HORIZONTALLY FROM FOUNDATION WALLS.
D. STORM AND WATER UTILITY PIPE INVERTS SHALL BE CONSTRUCTED WITHIN 0.10' OF DESIGN INVERT ELEVATIONS ASSUMING PIPE SLOPE AND SEPARATION IS MAINTAINED PER THE UTILITY DESIGN PLANS AND STATE REQUIREMENTS.
E. ALL UTILITIES SHALL BE INSTALLED WITH PLASTIC COATED TRACER WIRE (10 TO 14 GAUGE SOLID COPPER, OR COPPER COATED STEEL WIRE). PLASTIC WIRE MAY BE TAPED TO PLASTIC WATER OR SEWER PIPE. IF ATTACHED, THE TRACER WIRE SHALL BE SECURED EVERY 6 TO 20 FEET AND AT ALL BENDS. TRACER WIRE SHALL HAVE ACCESS POINTS AT LEAST EVERY 300 FEET.
F. ALL UTILITIES SHALL BE INSTALLED PER STATE, LOCAL, AND INDUSTRY STANDARDS. WATER, SANITARY, AND STORM SEWER SHALL BE INSTALLED PER "STANDARD SPECIFICATION FOR SEWER AND WATER CONSTRUCTION IN WISCONSIN". THE DESIGN ENGINEER SHALL BE RESPONSIBLE FOR OBTAINING STATE PLUMBING REVIEW APPROVAL. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL OTHER PERMITS REQUIRED TO INSTALL WATER, SANITARY AND STORM SEWER.
G. SEE PLANS FOR ALL OTHER UTILITY SPECIFICATIONS AND DETAILS.

CONSTRUCTION STAKING SERVICES

CONSTRUCTION STAKING SHALL BE COMPLETED BY EXCEL ENGINEERING AS REQUESTED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE. CONTRACTOR TO CONTACT RYAN WILGREEN AT 920-926-9800 TO GET STAKING PRICE TO INCLUDE IN BID TO OWNER. PAYMENT OF STAKING COSTS ABOVE AND BEYOND THE BASE PRICE DUE TO RESTAKING WILL BE THE RESPONSIBILITY OF THE CONTRACTOR, NOT THE OWNER. CAD DRAWING FILES AND SURVEY CONTROL WILL NOT BE PROVIDED FOR STAKING PURPOSES.

GENERAL PROJECT NOTES

1. UTILITY CONSTRUCTION TO BE CONSTRUCTED ACCORDING TO LOCAL ORDINANCES. CONTRACTOR TO OBTAIN ALL NECESSARY PERMITS.
2. CONTRACTOR SHALL CONTACT EXCEL ENGINEERING TO VERIFY THAT THE POND AND STORM SYSTEM HAS BEEN CONSTRUCTED ACCORDING TO PLANS. THE COST FOR THIS WORK SHALL BE PAID BY THE CONTRACTOR UNLESS OTHER ARRANGEMENTS ARE MADE. IF CHANGES ARE REQUIRED, CONTRACTOR SHALL PAY FOR ANY REWORK NECESSARY AND FOR THE AS-BUILT SURVEY NECESSARY FOR FINAL VERIFICATION.

NOTE:
PROPERTY LINES AND EASEMENTS SHOWN ON THIS PLAN WERE DRAFTED FROM INFORMATION CONTAINED IN TITLE COMMITMENT NO. 1055270L, BY KNIGHT BARRY TITLE GROUP, DATED AUGUST 7, 2019. AN UPDATED SURVEY HAS NOT BEEN AUTHORIZED AND THE PROPERTY LINES SHOWN ON THIS PLAN MAY BE INCOMPLETE OR IN ERROR.

NOTE:
EXISTING UTILITIES SHOWN ARE INDICATED IN ACCORDANCE WITH AVAILABLE RECORDS AND FIELD MEASUREMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING EXACT LOCATIONS AND ELEVATIONS OF ALL UTILITIES, INCLUDING SEWER AND WATER FROM THE OWNERS OF THE RESPECTIVE UTILITIES. ALL UTILITY OWNERS SHALL BE NOTIFIED BY THE CONTRACTOR 72 HOURS PRIOR TO EXCAVATION.

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PROPOSED FOR:
J.J. COOLERS, LLC.
3950 COUNTY ROAD PP
PLYMOUTH, WI 53073

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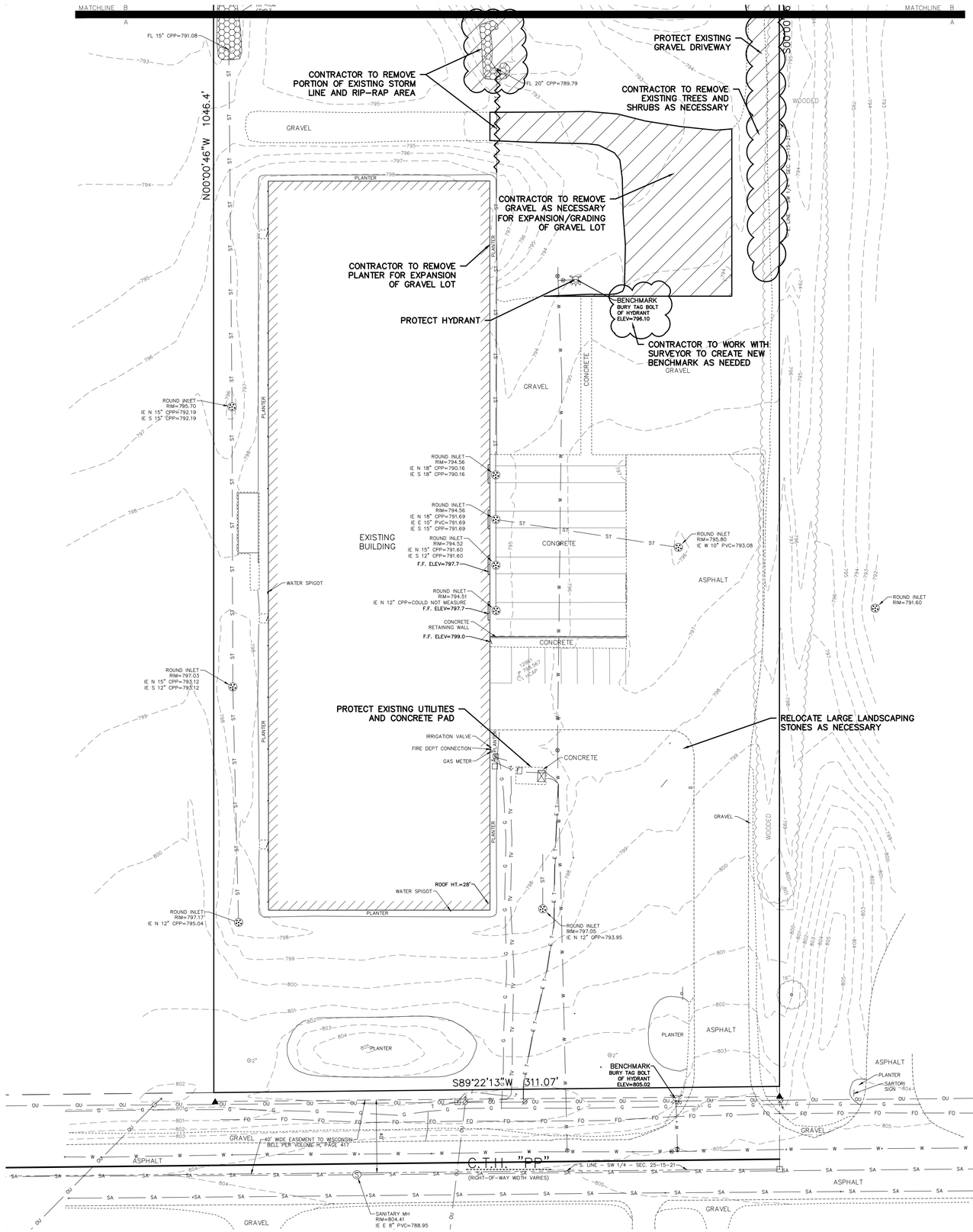
PROJECT MANAGER:	D.SCHWALBE
DESIGNER:	EXCEL ENGINEERING, INC
DRAWN BY:	E.E.I. 250226900
EXPEDITOR:	S. TIM
SUPERVISOR:	J. VAN ZEELAND
PRELIMINARY NO:	-------
CONTRACT NO:	89872
DATE:	JULY 24, 2025
SHEET:	C1.0

CIVIL COVER AND SPECIFICATION SHEET
EXCEL JOB #: 250226900
ISSUED FOR CONSTRUCTION



EXISTING BENCHMARK

BENCHMARKS FROM 2019. PRIOR TO CONSTRUCTION FIELD VERIFY BENCHMARKS. NOTIFY ENGINEER OF ANY DISCREPANCIES.

EXISTING UTILITIES

CONTRACTOR TO FIELD VERIFY SIZE, LOCATION, AND DEPTH OF ALL EXISTING UTILITIES ON SITE PRIOR TO CONSTRUCTION.

EXISTING SITE AND DEMOLITION PLAN

EXCEL JOB #: 250226900

NORTH

1"= 30'

SCALE 30' 0 30' 60' FEET

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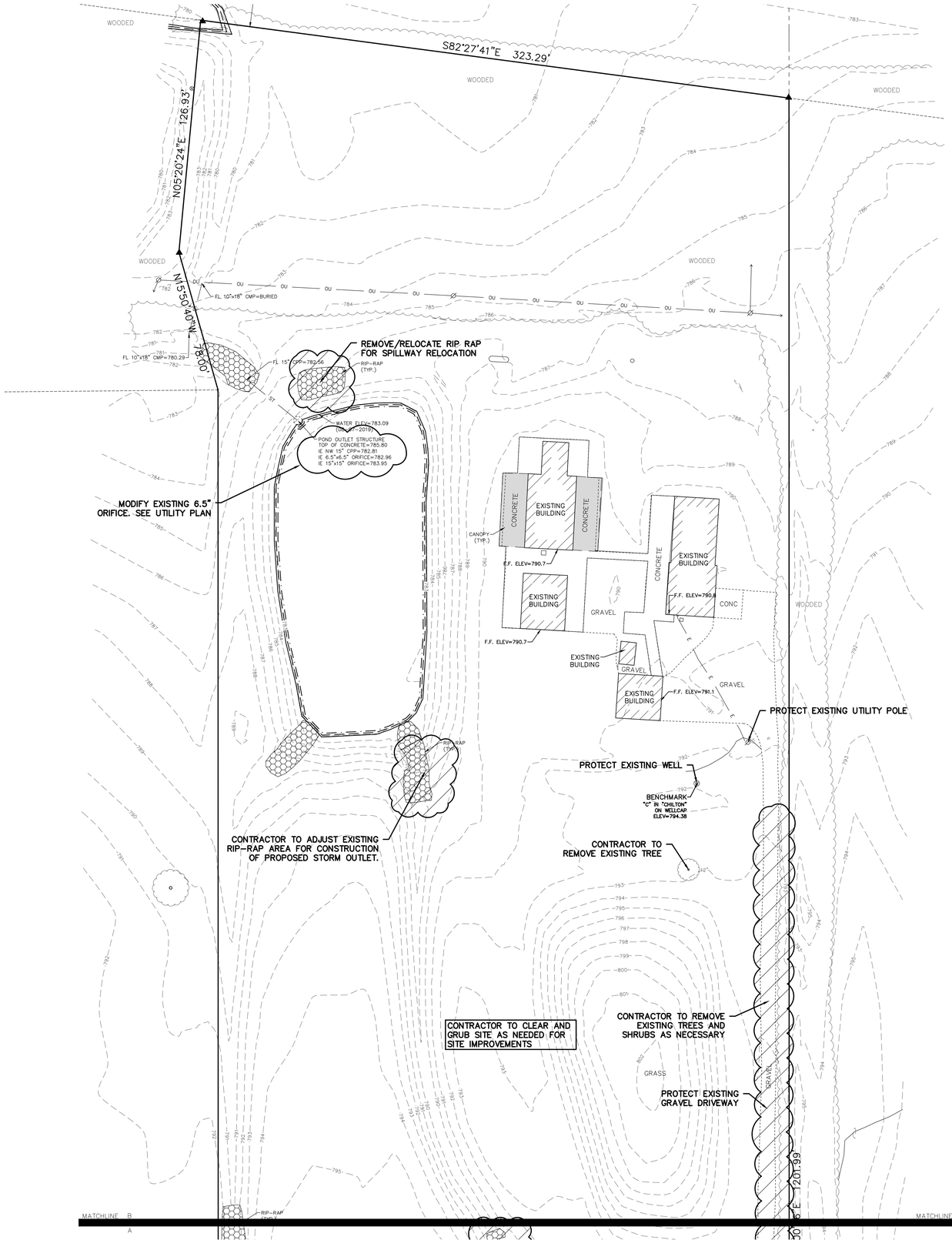

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EXISTING UTILITIES

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EXISTING SITE AND DEMOLITION PLAN

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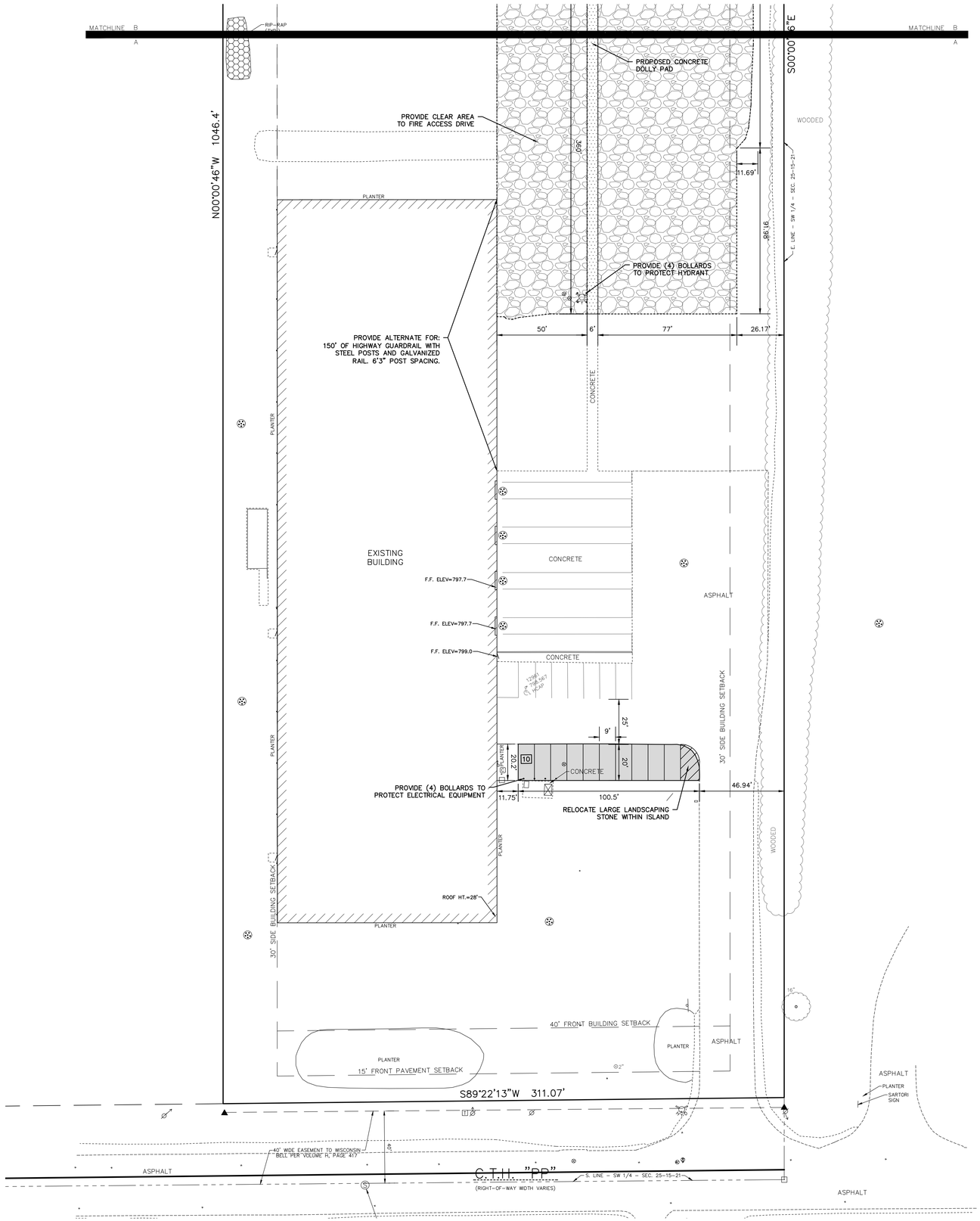
NORTH

1"= 30'

SCALE: 30' 0 30' 60' FEET

ISSUED FOR CONSTRUCTION



SITE INFORMATION:

ADDRESS:	3950 CTY RD. PP, PLYMOUTH, WI
PROPERTY AREA:	AREA = 383,544 S.F. (8.80 ACRES)
EXISTING ZONING:	HEAVY INDUSTRIAL
PROPOSED ZONING:	HEAVY INDUSTRIAL
EXISTING USE:	COOLER WAREHOUSE
PROPOSED USE:	COOLER WAREHOUSE

AREA OF SITE DISTURBANCE: 71,033 SF (1.63 ACRES)

SETBACKS:
- BUILDING: FRONT = 40' / SIDE = 30' / REAR = 40'
- PAVEMENT: FRONT = 15' / SIDE = 0' / REAR = 0'

BUILDING HEIGHT: 26'-8" (MAX. HEIGHT 45')

PARKING REQUIRED: 1 SPACE PER 2 EMPLOYEES. (2 SPACES REQ.)

PARKING PROVIDED: 17 SPACES (1 H.C. ACCESSIBLE)

HANDICAP STALLS REQUIRED: 1, HANDICAP STALLS PROVIDED: 1

LANDSCAPE REQUIREMENTS: MAXIMUM LOT COVERAGE – BUILDING ONLY: 50%

EXISTING SITE DATA

	AREA (AC)	AREA (SF)	RATIO
PROJECT SITE	8.80	383544	
BUILDING FLOOR AREA	1.20	52,485	13.7%
PAVEMENT (ASP. GRAVEL & CONC.)	1.43	62,479	16.3%
TOTAL IMPERVIOUS	2.64	114,964	30.0%
LANDSCAPE/ OPEN SPACE	6.17	268,580	70.0%

PREVIOUSLY PROPOSED PHASE 2 SITE DATA

	AREA (AC)	AREA (SF)	RATIO
PROJECT SITE	8.80	383,544	
BUILDING FLOOR AREA	2.05	89,440	23.3%
PAVEMENT (ASP. GRAVEL & CONC.)	2.19	95,578	24.9%
TOTAL IMPERVIOUS	4.25	185,018	48.2%
LANDSCAPE/ OPEN SPACE	4.56	198,526	51.8%

PROPOSED SITE DATA

	AREA (AC)	AREA (SF)	RATIO
PROJECT SITE	8.80	383,544	
BUILDING FLOOR AREA	1.20	52,485	13.7%
PAVEMENT (ASP. GRAVEL & CONC.)	2.53	110,280	28.8%
TOTAL IMPERVIOUS	3.74	162,765	42.4%
LANDSCAPE/ OPEN SPACE	5.07	220,779	57.6%

LEGEND:

HATCH	PAVEMENT SECTION
	STANDARD ASPHALT
	DOLLY PAD CONCRETE
	GRAVEL

SITE PLAN

EXCEL JOB #: 250226900

NORTH 1"= 30' SCALE 30' 0 30' 60' FEET

ISSUED FOR CONSTRUCTION

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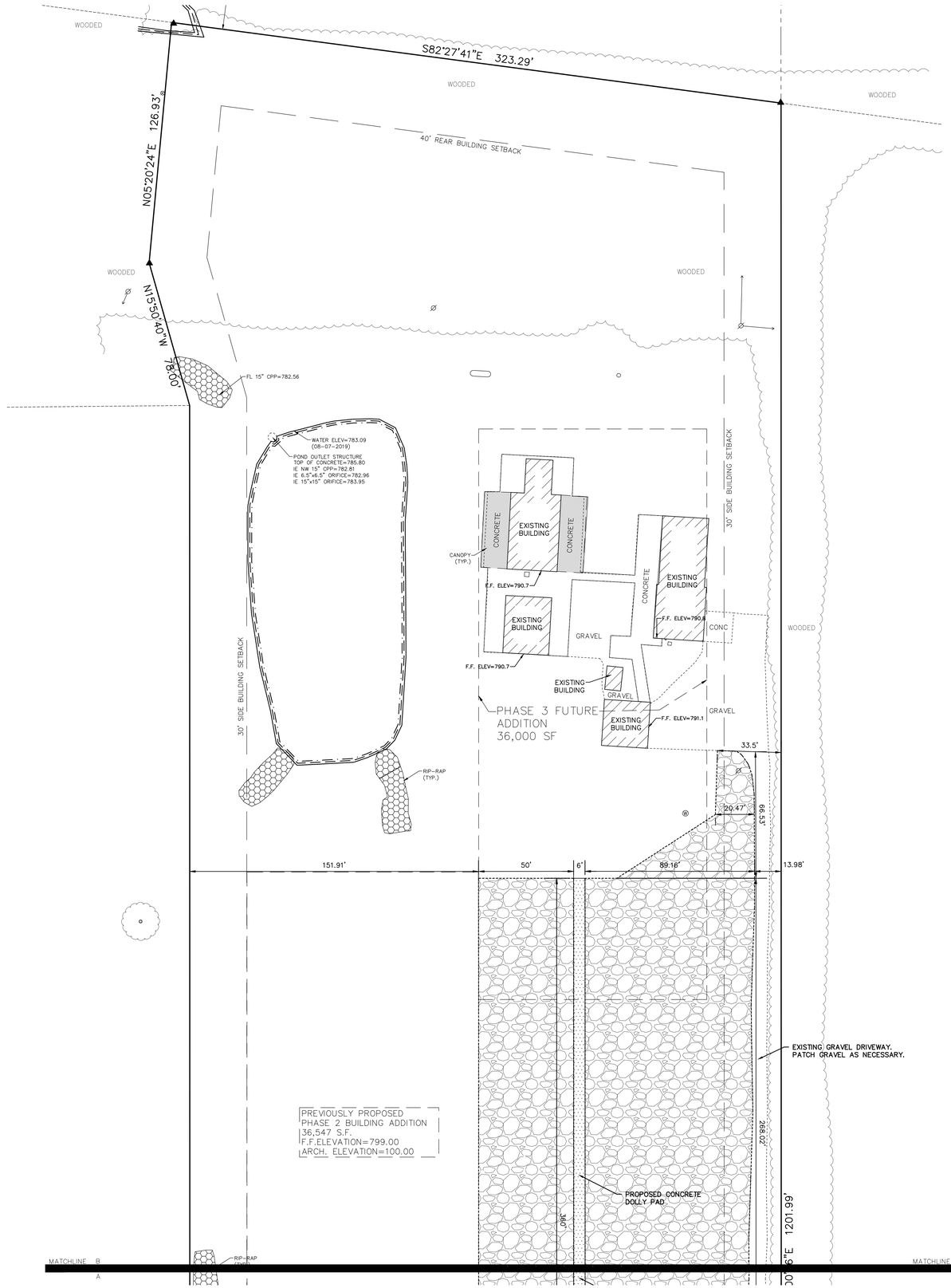

LEGEND:	
HATCH	PAVEMENT SECTION
	STANDARD ASPHALT
	DOLLY PAD CONCRETE
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SITE PLAN

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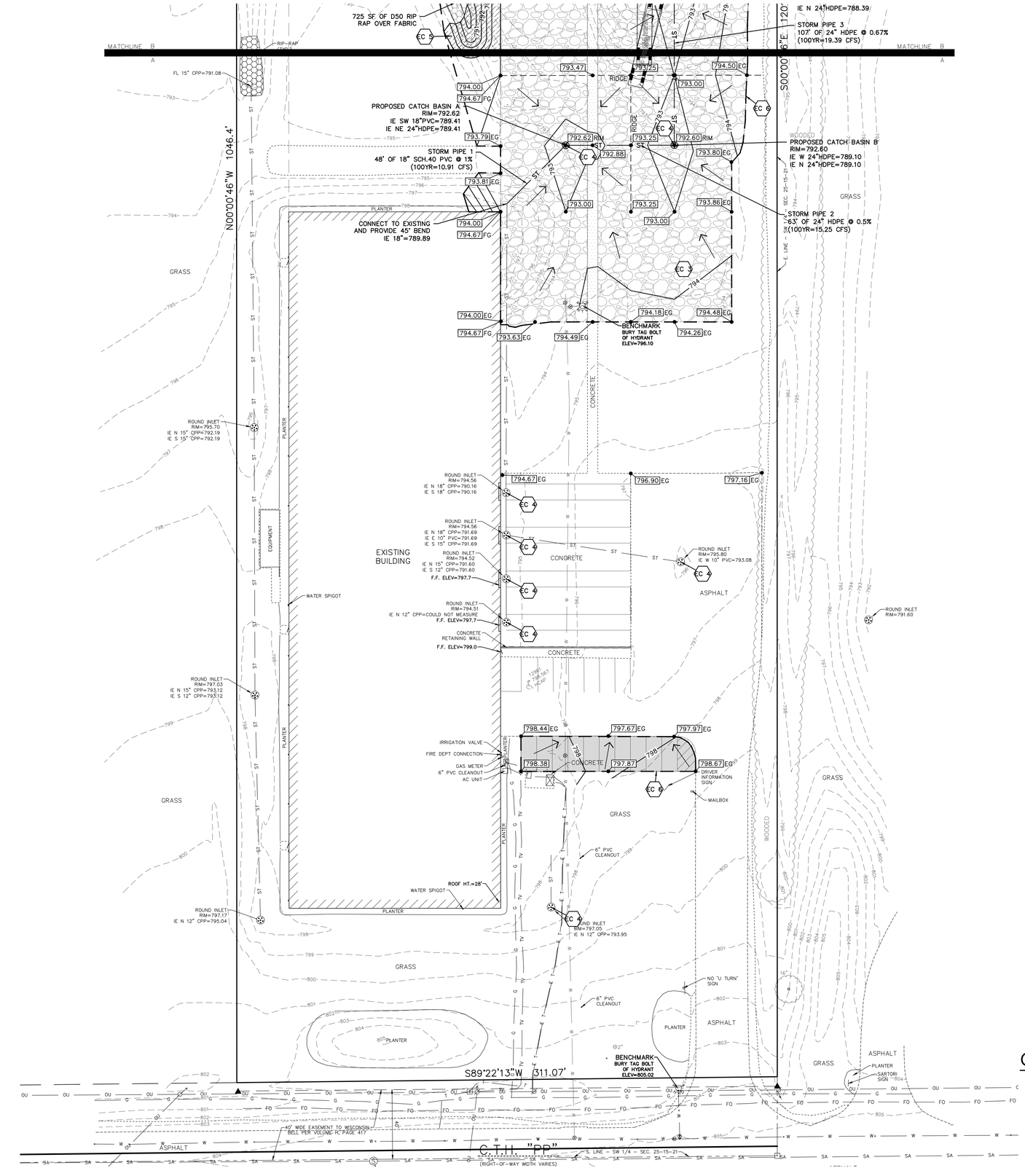

KEYNOTES	
EC 1	SILT FENCE
EC 2	DITCH CHECK
EC 3	STABILIZED CONSTRUCTION ENTRANCE (FINAL LOCATION BY GENERAL CONTRACTOR)
EC 4	INLET PROTECTION
EC 5	EROSION MATTING
EC 6	AREA OF SITE DISTURBANCE

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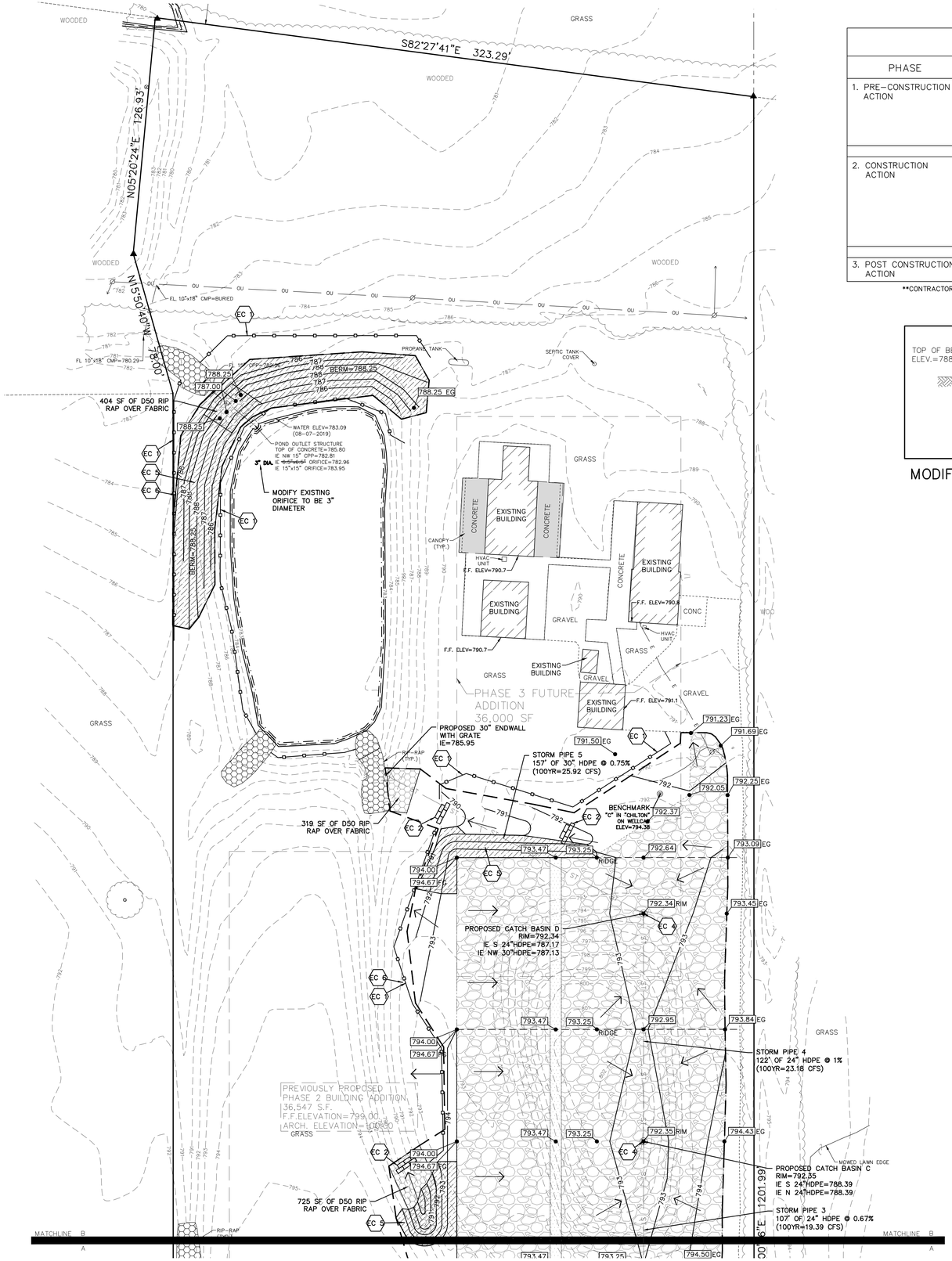
GRADING, EROSION CONTROL, AND UTILITY PLAN

EXCEL JOB #: 250226900

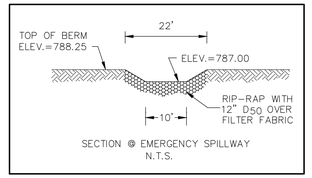
NORTH
1"= 30'
SCALE
30' 0 30' 60'
FEET

ISSUED FOR CONSTRUCTION



CONSTRUCTION SEQUENCE	
PHASE	TYPE OF ACTION
1. PRE-CONSTRUCTION ACTION	1. CONTRACTOR TO CALL DIGGERS HOTLINE AT A MINIMUM OF 3 DAYS PRIOR TO CONSTRUCTION.
	2. CONTRACTOR TO MAKE SURE THE EXISTING STORMWATER POND IS IN PLACE PER SURVEY BEFORE CONSTRUCTION CAN BEGIN.
	3. PLACE ALL SILT FENCE.
	4. CONSTRUCT TRACKING STONE ENTRANCES AND ANY TEMPORARY CONSTRUCTION ROADWAYS.
	5. CONSTRUCT PERMANENT RETENTION/DETENTION PONDS AND PERMANENT STORMWATER CONVEYANCE SYSTEMS.
	6. STABILIZE ALL TEMPORARY AND PERMANENT EROSION CONTROL AND STORMWATER CONVEYANCE SYSTEMS BEFORE TOPSOIL CAN BE STRIPPED.
2. CONSTRUCTION ACTION	1. CLEAR AND GRUB TREES AND BRUSH AND DEMO AS REQUIRED.
	2. STRIP AND RELOCATE TOPSOIL TO THE DESIGNATED TOPSOIL STOCKPILE. LOCATION BY OWNER, SURROUND WITH SILT FENCE.
	3. BEGIN MASS EARTH WORK FOR THE GRAVEL AND PAVEMENT AREAS.
	4. CONSTRUCT ANY REMAINING STORMWATER CONVEYANCE SYSTEMS, AND INSTALL ALL OTHER UTILITIES ON SITE.
	5. PLACE GRAVEL FOR ALL PROPOSED PAVEMENT AREAS, INCLUDING FIRE LANES.
	6. TOPSOIL, SEED, AND MULCH ALL DISTURBED AREAS OUTSIDE THE GRAVEL AND PAVEMENT AREAS.
	7. PAVE DRIVEWAYS AND PARKING AREAS.
	8. TOPSOIL, SEED, AND MULCH ALL OTHER DISTURBED AREAS. PLACE EROSION MATTING AND RIP RAP.
3. POST CONSTRUCTION ACTION	1. CONTRACTOR TO REMOVE TEMPORARY EROSION CONTROL MEASURES UPON SITE STABILIZATION.
	2. SEE THE POST CONSTRUCTION MAINTENANCE PLAN FOR PERMANENT STORMWATER MANAGEMENT SYSTEMS.

CONTRACTOR TO FOLLOW THE EROSION CONTROL SPECIFICATIONS FOR CONSTRUCTION EROSION CONTROL INSPECTION AND MAINTENANCE.



MODIFIED NORTH EMERGENCY SPILLWAY DETAIL

NO SCALE

NOTES:

IF GROUND WATER IS PRESENT; TILE SYSTEM SHOULD BE INSTALLED UNDER POND TO ALLOW PUMPING DURING CONSTRUCTION.

A LINING BASE SHALL BE PREPARED ON THE BOTTOM AND SLOPES OF THE AREA ARE TO BE LINED.

LINING BASE MATERIAL SHALL BE FREE OF ALL SHARP OBJECTS, ROOTS GRASS AND VEGETATION.

THE BASE MATERIAL SHALL BE NATIVE MATERIALS OR MATERIALS OBTAINED FROM A BORROW SOURCE COMPACTED TO A MIN. OF 95% COMPACTING OR AN APPROVED CONSTRUCTION FABRIC.

THE SUBGRADE SHALL BE PREPARED IMMEDIATELY PRIOR TO THE PLACING OF THE LINER. THE SURFACE ON WHICH THE LINER IS TO BE PLACED IS TO BE FIRM, CLEAN, DRY AND SMOOTH.

CONTRACTOR SHALL CONTACT EXCEL ENGINEERING PRIOR TO PLACEMENT OF POND LINER TO VERIFY THAT POND HAS BEEN CONSTRUCTED ACCORDING TO PLANS. IF CHANGES ARE REQUIRED, CONTRACTOR SHALL PAY FOR ANY REWORK NECESSARY AND FOR THE AS-BUILT SURVEY NECESSARY FOR FINAL VERIFICATION.

KEYNOTES	
EC 1	SILT FENCE
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GRADING, EROSION CONTROL, AND UTILITY PLAN

EXCEL JOB #: 250226900

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1"= 30' SCALE 30' 0 30' 60' FEET

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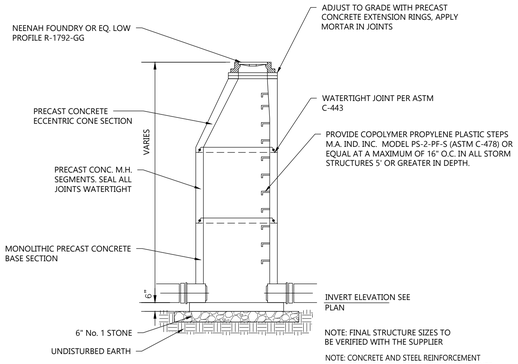
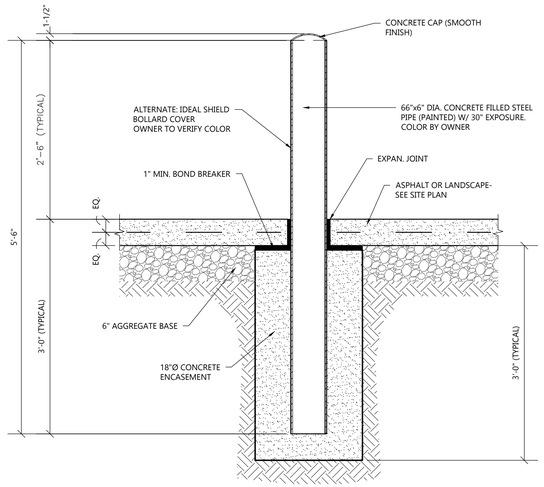


STORM CATCH BASIN W/ CONE SECTION DETAIL

NOT TO SCALE

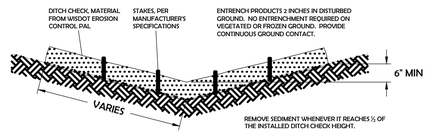


6" PIPE BOLLARD DETAIL

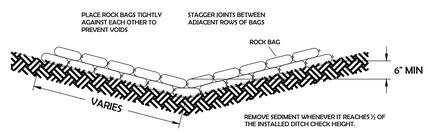
NOT TO SCALE

DITCH CHECK DETAIL

LOG-TYPE DITCH CHECK



ROCK BAG DITCH CHECK



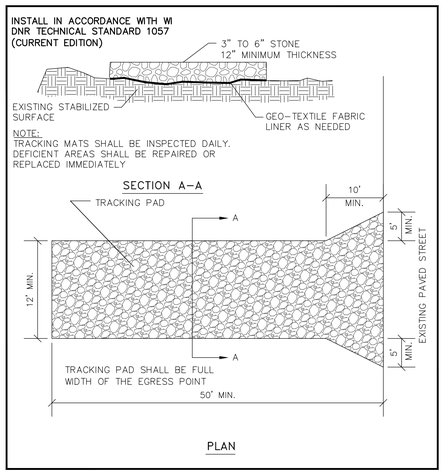
FILL BAGS WITH AGGREGATE CONFORMING TO THE FOLLOWING GRADATION (WSDOT NO. 1 COARSE AGGREGATE)

SIEVE	PERCENT PASSING BY WEIGHT
2-INCH	-
1½"-INCH	-
1-INCH	100
¾"-INCH	90-100
⅜"-INCH	20-55
No. 4	0-10
No. 8	0-5

PROVIDE BAGS MADE OF HIGH-DENSITY POLYETHYLENE OR GEOTEXTILE FABRIC

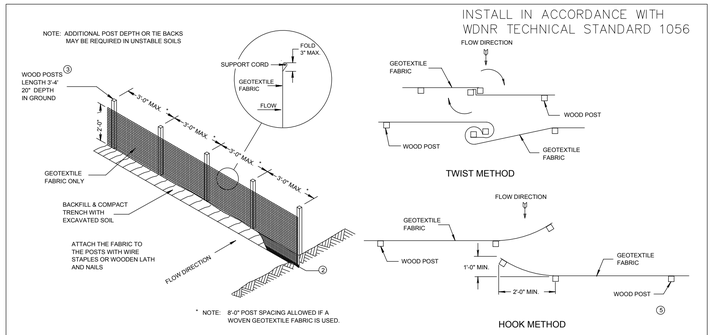
BAGS SHALL HAVE A MINIMUM IN-PLACE FILLED SIZE OF 18 INCHES LONG BY 12 INCHES WIDE BY 6 INCHES HIGH

INSTALL IN ACCORDANCE WITH WI DNR TECHNICAL STANDARD 1057 (CURRENT EDITION)

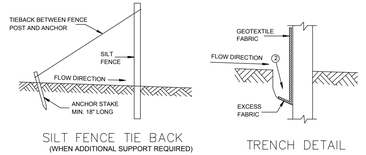


TRACKPAD DETAILS

NOT TO SCALE



SILT FENCE



SILT FENCE TIE BACK

(WHEN ADDITIONAL SUPPORT REQUIRED)

TRENCH DETAIL

JOINING TWO LENGTHS OF SILT FENCE

GENERAL NOTES

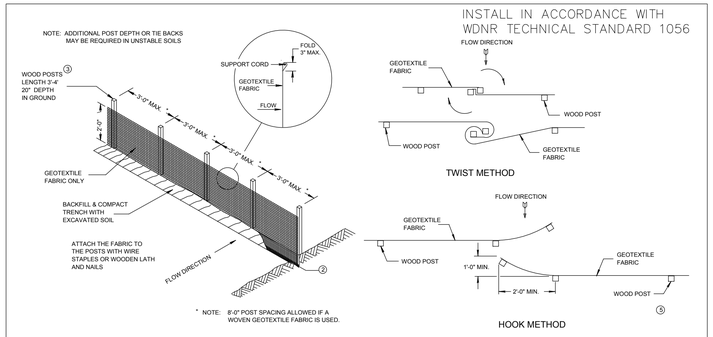
1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1" X 1" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS: A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

This drawing based on Wisconsin Department of Transportation Standard Detail Drawing 8 E 9-6.

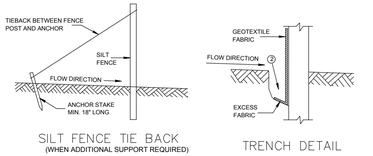
SILT FENCE

SILT FENCE – INSTALLATION DETAIL

NO SCALE



SILT FENCE



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SILT FENCE – INSTALLATION DETAIL

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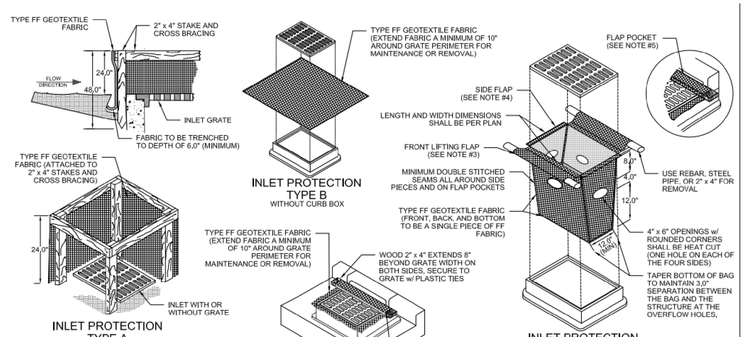


NOTES:

1. TAPER BOTTOM OF BAG TO MAINTAIN THREE INCHES OF CLEARANCE BETWEEN THE BAG AND THE STRUCTURE, MEASURED FROM THE BOTTOM OF THE OVERFLOW OPENINGS TO THE STRUCTURE WALL.
2. GEOTEXTILE FABRIC TYPE FF FOR FLAPS, TOP AND BOTTOM OF OUTSIDE OF FILTER BAG, FRONT, BACK, AND BOTTOM OF FILTER BAG BEING ONE PIECE.
3. FRONT LIFTING FLAP IS TO BE USED WHEN REMOVING AND MAINTAINING FILTER BAG.
4. SIDE FLAPS SHALL BE A MAXIMUM OF TWO INCHES LONG. FOLD THE FABRIC OVER AND REINFORCE WITH MULTIPLE STITCHES.
5. FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2" x 4", THE REBAR, STEEL PIPE, OR WOOD SHALL BE INSTALLED IN THE REAR FLAP AND SHALL NOT BLOCK THE TOP HALF OF THE CURB FACE OPENING.

MAINTENANCE NOTES:

1. WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED IN THE FABRIC DOES NOT FALL INTO THE STRUCTURE. MATERIAL THAT HAS FALLEN INTO THE INLET SHALL BE IMMEDIATELY REMOVED.

WISCONSIN DEPT. OF NATURAL RESOURCES

1060
TECHNICAL STANDARD No.
01/2014
REVISION DATE
NOT TO SCALE

INLET PROTECTION DETAILS

DETAILS

EXCEL JOB #: 250226900

ISSUED FOR CONSTRUCTION

Keller

PLANNERS | ARCHITECTS | BUILDERS

FOX CITIES
N216 State Road 55
P.O. Box 620
Kaukauna, WI 54130
PHONE (920) 766-5795 / 1-800-236-2534
FAX (920) 766-5004

MADISON
711 Lois Drive
Sun Prairie, WI 53590
PHONE (608) 318-2336

MILWAUKEE
W204 N11509
Goldendale Rd
Germantown, WI 53022
PHONE (262) 250-9710 1-800-236-2534
FAX (262) 250-9740

WAUSAU
5605 Lilac Ave
Wausau, WI 54401
PHONE (715) 849-3141
FAX (715) 849-3181

www.kellerbuilds.com

PROPOSED FOR:

J.J. COOLERS, LLC.

3950 COUNTY ROAD PP
PLYMOUTH, WI 53073

REVISIONS

1. .
2. .
3. .
4. .
5. .
6. .

PROJECT MANAGER: D.SCHWALBE

DESIGNER: EXCEL ENGINEERING, INC

DRAWN BY: E.E.I. 250226900

EXPEDITOR: S. TIM

SUPERVISOR: J. VAN ZEELAND

PRELIMINARY NO: _____

CONTRACT NO: 89872

DATE: JULY 24, 2025

SHEET: C2.0

City of Plymouth
128 Smith St. - P.O. Box 107
Plymouth, WI 53073-0107

Telephone: **(920) 893-3745**
Facsimile: **(920) 893-0183**
Web Site: **plymouthgov.com**

DATE: August 25, 2025

TO: Plan Commission

FROM: Jack Johnston, Assistant City Administrator/Community Development Director

RE: **Site Plan Seeking Approval:** Parcel number 59271822750 (southeast corner of STH 57 and CTH PP), Sargento Cheese Inc, regarding minor revisions to previously approved site plans for new ~384,000 square foot industrial facility in the H-I Heavy Industrial zoning district.

Traffic Impact Analysis Review and Approval: For the new Sargento facility on the corner of STH 57 & CTH PP. Review and approval of TIA by Plan Commission required as part of contingent approval of site plan received at July 10, 2025 Plan Commission meeting.

Background:
At the July 10, 2025 Plan Commission meeting, Sargento received contingent approval for their new ~384,000 square foot facility to be located on the corner of STH 57 and CTH PP in the City of Plymouth. One of those contingencies was a traffic impact analysis (TIA) being completed and reviewed by City staff and approved by the Plan Commission at a future meeting.

The TIA has been completed by Traffic Analysis & Design, Inc (TADI) out of Cedarburg, Wisconsin and furnished to the City. Staff has since worked with the Kapur to conduct an independent third-party review of the TIA to ensure its completeness and accuracy. Kapur's findings were that the TIA meets industry standard and is acceptable.

Additionally, Sargento has since requested consideration of a very minor revision to their site plan after reevaluating the vehicle circulation of the site. Their engineers have determined that they need to reverse the truck and personnel entry drives off of CTH PP. These drives will not move from their original location on CTH PP, they will just be designated differently to accommodate their respective traffic. There are also minor revisions within the site which will be covered in more detail later in this report, but no changes to the actual building. The TIA submitted for review accounts for the driveway switch.

Site Plan Changes:
In association with the driveway access change, the following changes are included in the revised site plan from what was previously approved:

- Truck entry will be west entry at County Road PP to allow a counterclockwise on site movement of incoming trucks
- Personal Vehicle Parking lot shifted east to accommodate the truck drive and turns
- Personal Vehicle entry will be east entry at County Road PP

- Pump house relocated to west side of site along truck drive
- Emergency access road from parking lot to complete full access around site relocated to east side
- Relocation of Sanitary Sewer west to align with the other City Sanitary Sewer connection location
- Reduction in permeable surfaces as follows:
    - Reduction in Concrete Sidewalk Pavement along North of building to 3,585 sq ft - reduction of approximately 800 sq ft
    - Reduction in Light Duty Asphalt Pavement to 129,100 sq ft - reduction of approximately 25,700 sq ft
    - Reduction in Heavy Duty Asphalt Pavement to 153,575 sq ft - reduction of approximately 3,275 sq ft

TIA Findings:
The furnished TIA can be summarized with the following excerpt from the report:

- The proposed development is not expected to significantly impact traffic operations at the study intersections [STH 57 and CTH PP to the west]. Delay and queue increases are expected to be minimal with no changes in Levels of Service. The eastbound right-turning movement volumes on CTH PP at the development access driveway are high enough to warrant a dedicated right-turn lane on CTH PP at the east driveway. A dedicated right-turn lane at the west driveway is also recommended due to truck traffic exiting the high speed CTH PP facility. The westbound left-turning movement volumes are not high enough to warrant a left-turn lane on CTH PP.

Note: The needed turn lanes on CTH PP as well as the driveway access are permitted by Sheboygan County. Road infrastructure improvements on City roads are not anticipated as part of this project.

City Staff Comment:
Both the Department of Public Works and Plymouth Fire Department have reviewed the proposed minor site plan change and have no concerns.

The TIA was sent to Kapur Engineering on behalf of the City of Plymouth for third party review and analysis. Kapur provided a comment letter on their analysis of the TIA and will be present at the meeting for any questions of the Commission.

Staff Recommendation:
Staff recommends the Plan Commission approve site plan revisions and the findings of the TIA as presented. These should be done via separate motions.

Attachments:

I. Revised Site Plan
II. Traffic Impact Analysis (TIA)
III. Kapur Review Comment Letter

Action	Date	Status
Rezone/Comp Plan Amendment Plan Commission Meeting	4/3/2025	Rec. Approval
Comp Plan Public Notice in *Plymouth Review*	4/11/2025	Published
Rezone Public Notice in *Plymouth Review*	4/25, 5/2	Published
Mailer to nearby municipalities within 1,000' for affected area	5/1/2025	Mailed
Rezone and Comp Plan Public Hearing at Common Council	5/13/2025	Approved
Common Council Final Action on Rezone and Comp Plan	5/13/2025	Approved
Plan Commission Site Plan Review	7/10/2025	Contingent Approval
Plan Commission TIA Review & Minor Site Plan Revision	8/7/2025	This Meeting

LIGHT DUTY CONCRETE PAVEMENT
HEAVY DUTY CONCRETE PAVEMENT
HEAVY DUTY ASPHALT PAVEMENT
LIGHT DUTY ASPHALT PAVEMENT
GRASS / LANDSCAPE AREA
CONSTRUCTION LAYDOWN AREA
CURB AND GUTTER
WDNR WETLAND AREA
ENVIRONMENTAL BOUNDARY AREA

LEGEND

BOUNDARY LINE
EASEMENT
FENCING
LANDSCAPING
SETBACK/BUFFER
TREE LINE
EX COMMUNICATIONS LINE
EX UNDERGROUND ELECTRIC
EX GAS LINE
EX OVERHEAD ELECTRIC
EX SANITARY SEWER
EX STORM SEWER
EX WATERLINE
CENTERLINE
RIGHT OF WAY

IRON PIN FOUND
IRON PIN SET
BENCHMARK
PIPE BOLLARD/ POST
FIRE HYDRANT
SIGN
SIGNAL POLE
UTILITY POLE
GAS VALVE
GAS METER
WATER METER
STAND PIPE
WATER VALVE
LIGHT POLE
GUY WIRE

PR COMMUNICATIONS LINE
PR UNDERGROUND ELECTRIC
PR NATURAL GAS LINE
PR LP GAS LINE
PR OVERHEAD ELECTRIC
PR SANITARY SEWER
PR SANITARY FORCE MAIN
PR STORM LINE
PR WATERLINE
PUMPED RECLAIMED WATER
PR FIRE PROTECTION LINE
PR FIRE PROTECTION FROM PUMP
CENTERLINE
RIGHT OF WAY

SANITARY MANHOLE
STORM MANHOLE
ELECTRIC MANHOLE
COMMUNICATIONS MANHOLE
MAILBOX
ELECTRIC TRANSFORMER
ELECTRIC METER
DECIDUOUS TREE
CONIFEROUS TREE
DROP BOX INLET
DROP BOX INLET
CURB BOX INLET
COMMUNICATIONS PEDESTAL
CLEAN OUT
HVAC UNIT

SITE PLAN NOTES:
- EXISTING PAVEMENT AND BOUNDARY LINES TO BE VERIFIED

PARKING
-- PROVIDED:
--- 350 TYP SPACES
--- 9 ADA SPACES
--- TOTAL: 341 SPACES

FUTURE PARKING
-- PROVIDED:
--- 48 FUTURE TYP SPOTS
--- 0 FUTURE ADA SPOTS
--- TOTAL FUTURE SPOTS: 389 SPOTS

0 80 160 Feet

STATE HIGHWAY 57
AS LAID OUT UNDER PROJECT T02-3(25) DATED 1/12/1961

57TH FIELD ARTILLERY BRIGADE MEMORIAL HIGHWAY

N 02° 11' 57" W 2297.00' (M)
S 87° 54' 27" W 1142.26' (M)
S 01° 40' 12" E 2297.21' (M)
TOTAL BOUNDARY AREA:
2775000 SQFT
63.705 AC.

STORM WATER POND
PARCEL A
TAX KEY: 59271030307
PARCEL B
TAX KEY: 59271030309

FUTURE IMPERVIOUS

BLDG FFE: 821.75'
PUMP HOUSE
JOCKEY SHED
Approx. 4'high x4'wide truck entrance sign
Approx. 8'high x12'wide Sargento monument sign

S02°11'57"E 199.93' (M)
S42°44'47"W 140.73' (M)
N 88° 01' 44" E 494.32' (M)
S87°48'14"W 392.46' (M)
N 01° 43' 21" W 301.50' (M)
N 87° 48' 14" E 371.98'
N 01° 43' 21" W 5.00' (M)
N87°48'14"E 419.67' (M)
N87°48'14"E 417.01'(M)
S 01° 30' 51" E 306.43' (M)
S01°07'20"E 346.5'(R)
N 87° 49' 35" E 253.24' (M)
S87°48'14"W 264.58' (M)

COUNTY ROAD JW

198.15'
136.50'
325.00
41.33
13.00
9.00
20.00
24.00
30.00
40.00
50.00
74.00
75.00
150.00
R75.00
R115.00
R40.00
R25.00
R15.00
R20.00
R5.00
R30.00
R98.50
R128.50
R100.00
R125.00

#	DATE	DESCRIPTION
13	07/11/2025	DOCUMENT RELEASE
12	06/27/2025	DOCUMENT RELEASE
10	05/09/2025	DOCUMENT RELEASE
09	03/05/2025	PROPOSAL DOCUMENTS
07	02/10/2025	PRICING DOCUMENTS

Gray
10 Gurnity Street
Lexington, Kentucky 40507-1430
Telephone: (859) 281-6000
Architecture | Interiors | Engineering
Associates | Consultants
Missouri | Kansas | Kentucky

Gray AES
GRAY AES, PSC
10 Gurnity Street
Lexington, Kentucky 40507-1430
Telephone: (859) 281-6000

A NEW PRODUCTION FACILITY FOR
PROJECT G4 - BUILDING FACILITY
PLYMOUTH, WISCONSIN

DRAWN BY	CHECKED BY	#
LS	BDD	#

JOB NUMBER
205118

OVERALL SITE LAYOUT PLAN G4

SHEET NUMBER
C-400

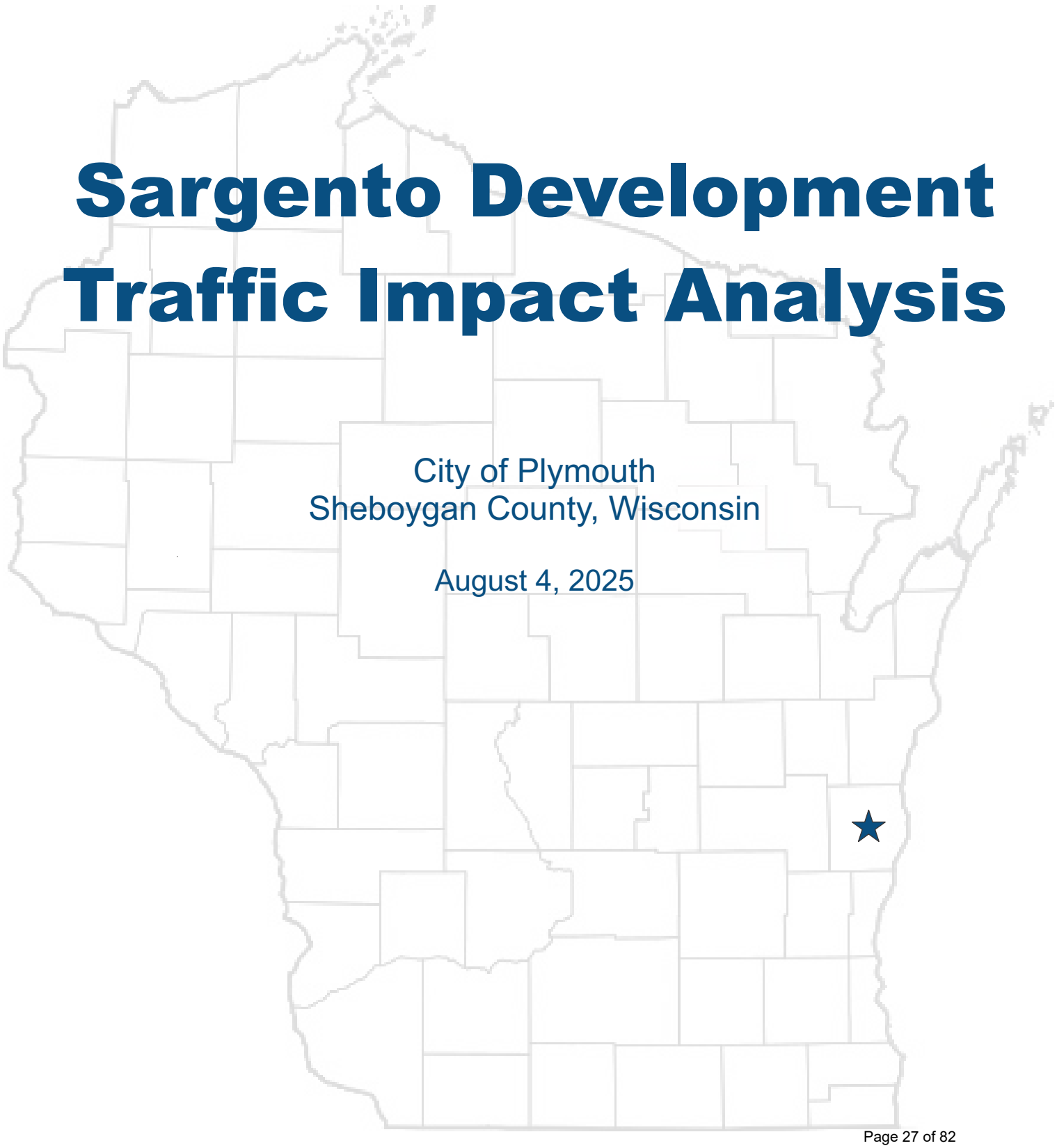
TRAFFIC ANALYSIS & DESIGN, INC.

P.O. Box 128
Cedarburg, WI 53012
800.605.3091
www.tadi-us.com

Sargento Development Traffic Impact Analysis

City of Plymouth
Sheboygan County, Wisconsin

August 4, 2025



TRAFFIC IMPACT ANALYSIS FOR:

SARGENTO DEVELOPMENT

CITY OF PLYMOUTH, SHEBOYGAN COUNTY, WISCONSIN

DATE SUBMITTED: August 4, 2025

PREPARED FOR:
Gray AES
10 Quality Street
Lexington, KY 40507-1450
Phone: (859) 474-8666
Contact Person: Troy Woodard

PREPARED BY:
Traffic Analysis & Design, Inc.
PO Box 128
Cedarburg, WI 53012
Phone: (800) 605-3091
Contact Persons: Don Lee, P.E. (WisDOT TIA Certification # SE05-804-046)
John Bieberitz, P.E., PTOE (WisDOT TIA Certification # SE05-804-044)

"I certify that this Traffic Impact Analysis has been prepared by me or under my immediate supervision and that I have experience and training in the field of traffic and transportation engineering."

_____
Donald J. Lee, P.E.
Wisconsin Registration #35214-006
Traffic Analysis & Design, Inc.

Sargento Development
Traffic Impact Analysis
Table of Contents

LIST OF EXHIBITS

LIST OF APPENDICES

CHAPTER I – INTRODUCTION & EXECUTIVE SUMMARY

PART A – PURPOSE OF REPORT AND STUDY OBJECTIVES

Sargento is proposing to construct a new manufacturing facility to be located on the south side of CTH PP immediately east of STH 57, in the City of Plymouth, Sheboygan County, Wisconsin. TADI performed this traffic impact analysis (TIA) to determine the expected weekday morning and evening peak hour operating conditions and recommendations at the study area intersections under the existing, opening year 2026 Full Build, and design year 2036 Full Build traffic conditions.

This report documents the procedures, findings, and conclusions of the analysis. The analysis identifies recommended modifications based on existing roadway conditions, existing traffic volumes, future projected traffic volumes and based on additional traffic expected to be generated by the proposed Sargento development.

PART B – EXECUTIVE SUMMARY

The executive summary includes a description of the study area, description of the proposed development and conclusions based on the findings of the TIA.

B1. Study Area

A study area map is shown in Exhibit 1-1. The study area includes the following existing intersections, noting that the node number refers to the reference number used in the modeling software.

- Node 100: CTH PP with STH 57 (existing traffic signal control)
- Node 200: CTH PP with Proposed West Driveway (proposed one-way stop control)
- Node 300: CTH PP with Toro Driveway/Proposed East Driveway (existing one-way stop control/proposed two-way stop control)

B2. On-Site Development Description

A conceptual development site plan is shown in Exhibit 1-2. A 384,000-square-foot manufacturing facility with about 115 employees is proposed to be constructed on the vacant lot. The employee count includes about 25 office/support staff. The facility is expected to operate under three shifts with the first shift from 6:00am to 2:00pm, second shift from 2:00 to 10:00pm and third shift from 10:00pm to 6:00am. According to the owners, about 40 to 50 trucks are expected on the site per day.

For the purpose of the TIA, the full build completion of the development was assumed to occur in Year 2026. Therefore, full build out of the Sargento development is included in the Year 2026 Full Build and Year 2036 Full Build traffic scenarios. If the facility expands in the future, it is understood that a future TIA would be required for the expansion plan.

B3. Off-Site Development Description

No off-site developments were identified within the limits of the study area.

B4. Site Generated Traffic

To address any potential future traffic impacts along study area roadways and at the intersections adjacent to the development, it is necessary to identify the hourly and daily volume of traffic generated by the proposed development. The traffic volumes expected to be generated by the proposed developments are based on the size and type of the proposed uses, and on expected employee volumes working at the facility. Since the shift changes are expected to take place off

peak hour from the adjacent street traffic peak hours, the office/support staff counts were used to determine the traffic expected during the adjacent street traffic peak hours.

Proposed Sargento Development Trip Generation
The proposed Sargento development is expected to generate approximately 30 new trips (25 in/5 out) during the typical weekday morning peak hour, approximately 30 new trips (5 in/25 out) during the typical weekday evening peak hour, and approximately 850 new trips over the course of a typical weekday (425 in/425 out) under full build conditions. Calculations are shown at the bottom of Exhibit 4-2.

B5. Proposed Access

As the conceptual site plan shows, access to the Sargento development is proposed via two access driveways along CTH PP. The "East Driveway" is proposed to be located opposite the existing Toro driveway and is expected to be used for employee and visitor access. The "West Driveway" is proposed to be located about 200-feet west of the "East Driveway" and is expected to be used for truck access. Parking for employees and visitors is proposed on the north side of the site and truck docks are proposed on the east and west sides of the site.

B6. Recommended Modifications

Recommended modifications, shown in Exhibit 1-3, are split into the following categories:

- "Existing Traffic" – These modifications are expected to be necessary to accommodate the Year 2025 Existing traffic volumes without the proposed development.
- "Background Traffic" – These modifications are expected to be necessary to accommodate the Year 2036 Background traffic without the proposed development, which includes the modifications required from general background growth in the study area through the year 2036. The background traffic recommended modifications are in addition to conditions as they currently exist.
- "Full Build Traffic" – These modifications are expected to be necessary to accommodate the Year 2036 Full Build traffic volumes which includes full build out of the proposed Sargento development as well as background traffic growth in the study area through the year 2036. Full Build traffic modifications are in addition to background traffic recommended modifications. Note that the modifications recommended are for both the year 2026 and the year 2036.

The analysis was conducted using existing intersection geometrics and traffic control and the existing traffic signal timings. The following modifications, as shown in Exhibit 1-3, are recommended to accommodate the Year 2025 Existing, Year 2036 Background and Year 2036 Full Build traffic volumes, respectively. *Modifications are for jurisdictional consideration and are not legally binding. The City of Plymouth and Sheboygan County reserve the right to determine alternative solutions.*

Node 100: CTH PP & STH 57

- *Existing Traffic:* No modifications.
- *Background Traffic:* No modifications.
- *Full Build Traffic:* No modifications.

Node 200: CTH PP & Proposed West Driveway

- *Existing Traffic:* No modifications.
- *Background Traffic:* No modifications.

- *Full Build Traffic:*
    - Provide a single lane driveway on the south approach as shown on the conceptual site plan.
    - Construct a dedicated right-turn lane on the west approach (distance shown on Exhibit 1-3).
    - Provide stop sign control on the south approach.

Node 300: CTH PP & Toro Driveway/Proposed East Driveway

- *Existing Traffic:* No modifications.
- *Background Traffic:* No modifications.
- *Full Build Traffic:*
    - Provide a single lane driveway on the south approach as shown on the conceptual site plan.
    - Construct a dedicated right-turn lane on the west approach (construct up to and through the west driveway).
    - Provide stop sign control on the south approach.

In addition to the typical weekday morning and weekday afternoon commuter peak hour analysis, a separate afternoon shift change off peak hour sensitivity analysis was also completed for the study area intersections (calculations and exhibits shown in Appendix E). Due to the shift changes occurring during the non-peak hours on CTH PP and STH 57, when traffic volumes on CTH PP and STH 57 are lower, the separate sensitivity analysis was conducted during the 1:30 to 2:30pm hour, where the 1st shift employees are leaving and the 2nd shift employees are entering. Based on a review of the historic WisDOT hourly traffic volumes on CTH PP and STH 57, the 1:00 to 2:00pm and 2:00 to 3:00pm traffic volumes are approximately 15- to 10-percent lower than the 3:00 to 4:00pm peak hour traffic volumes, respectively. Therefore, the Year 2036 3:00 to 4:00pm existing/background peak hour traffic volumes were reduced by 10-percent to represent the 1:30 to 2:30pm shift change hour. During this hour and with the shift change, 90 vehicles are expected to enter the proposed Sargento facility, and 90 vehicles are expected to exit, using the same percentage distribution as described later in this report. This additional nonpeak hour shift change traffic was added to the adjusted 1:30 to 2:30pm adjusted traffic turning movements to determine the total traffic volumes. This sensitivity analysis traffic was analyzed for the Year 2036 full build (1:30 shift change) conditions which resulted in LOS C or better for all turning movements and all queues expected to fall within the existing turn lane and painted taper lengths. It is noted that the overall sensitivity analysis traffic volumes with the shift change of 1:30 to 2:30pm are less than the Year 2036 full build traffic volumes from 3:00 to 4:00pm peak hour traffic. The sensitivity analysis traffic shift change off peak hour (1:30 to 2:30pm) Year 2036 Build traffic volumes at the CTH PP driveway were utilized to determine if left or right turn lanes are warranted on CTH PP.

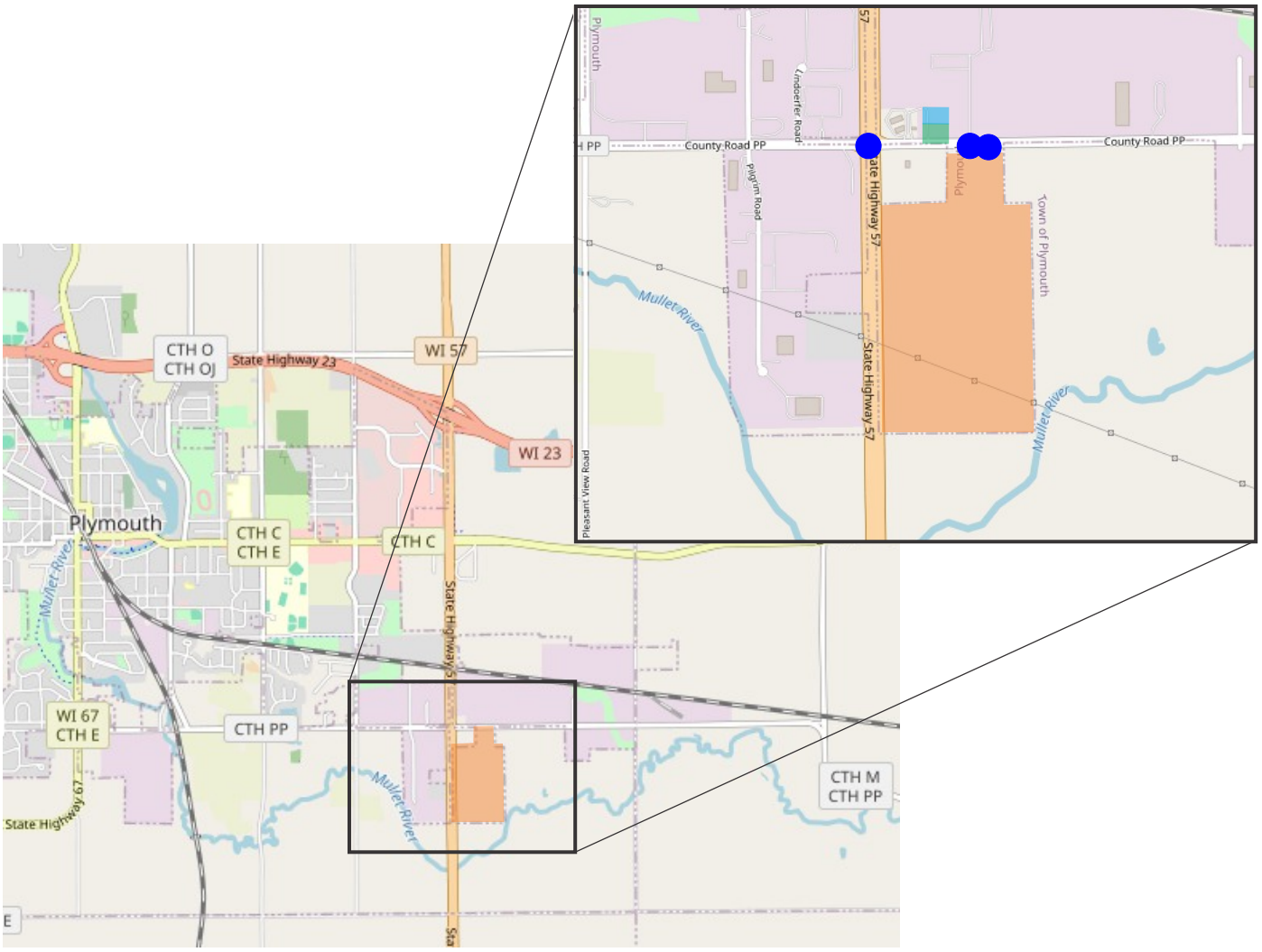
A separate afternoon sensitivity analysis was also completed for the study area intersections assuming the afternoon shift change would occur during the typical peak hour of adjacent street traffic, or 3:00 to 4:00pm (calculations and exhibits shown in Appendix F). During this hour and with the shift change, 90 vehicles are expected to enter the proposed Sargento facility, and 90 vehicles are expected to exit, as described in the previous paragraph. This additional peak hour shift change traffic was added to the 3:00 to 4:00pm peak hour traffic turning movements to determine the total traffic volumes. This sensitivity analysis traffic was analyzed for the Year

2036 full build (3:00pm shift change) conditions which resulted in LOS C or better for all turning movements and all queues expected to fall within the existing turn lane and painted taper lengths. The sensitivity analysis traffic shift change peak hour (3:00 to 4:00pm) Year 2036 Build traffic volumes at the CTH PP driveway were also utilized to determine if left or right turn lanes are warranted on CTH PP.

The proposed development is not expected to significantly impact traffic operations at the study intersections. Delay and queue increases are expected to be minimal with no changes in Levels of Service. As described above, the eastbound right-turning movement volumes on CTH PP at the development access driveway are high enough to warrant a dedicated right-turn lane on CTH PP at the east driveway. A dedicated right-turn lane at the west driveway is also recommended due to truck traffic exiting the high speed CTH PP facility. The westbound left-turning movement volumes are not high enough to warrant a left-turn lane on CTH PP.

B7. Conclusion

The study area intersections are expected to operate desirably at LOS C or better conditions with the anticipated development and with the identified recommended modifications if properly designed and implemented through the design year 2036.



LEGEND

- Study Area Intersection
- On-Site Development

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EXHIBIT 1-1
PROJECT OVERVIEW MAP

PLYMOUTH, WISCONSIN

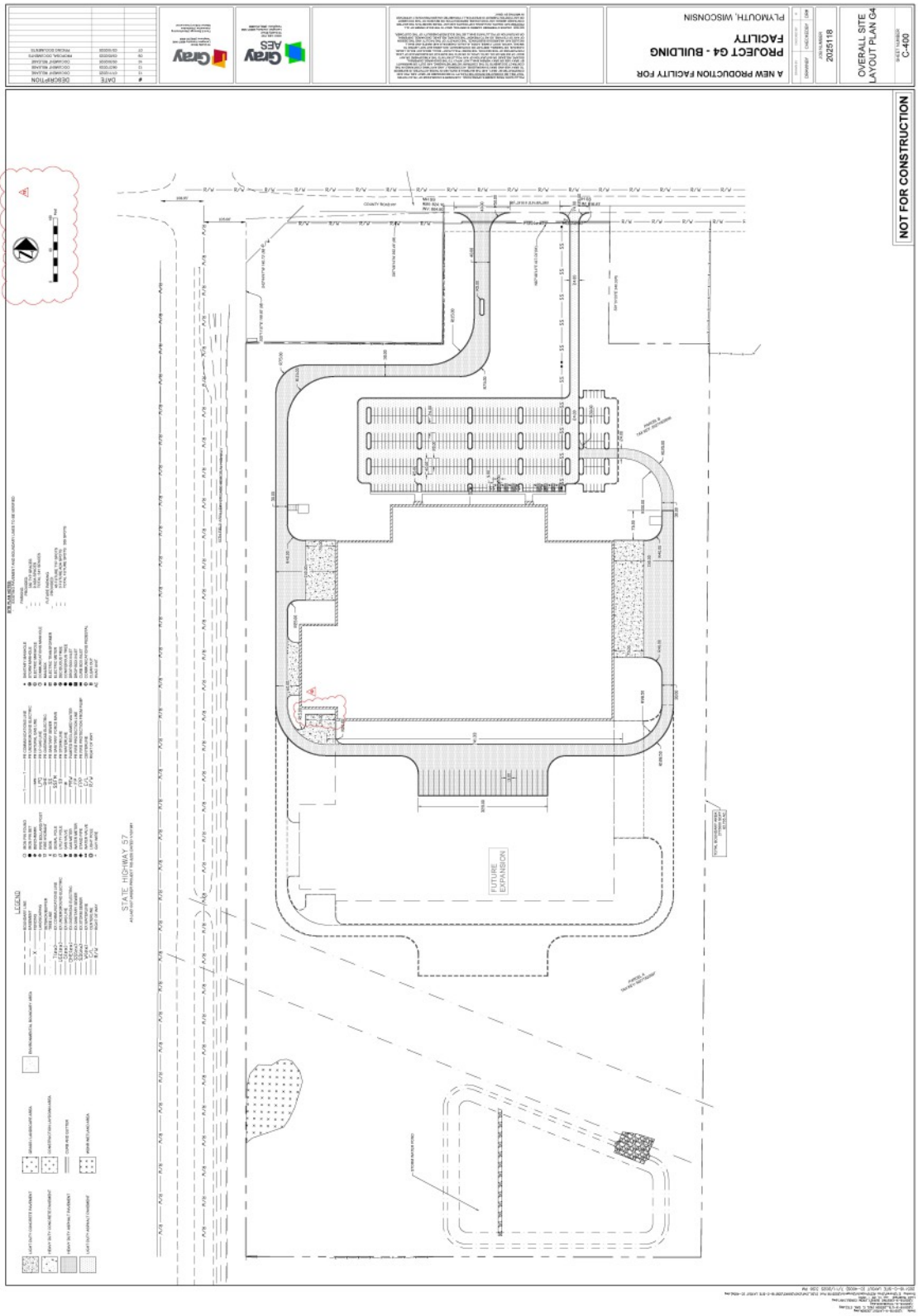

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EXHIBIT 1-2
CONCEPTUAL SITE PLAN

PLYMOUTH, WISCONSIN

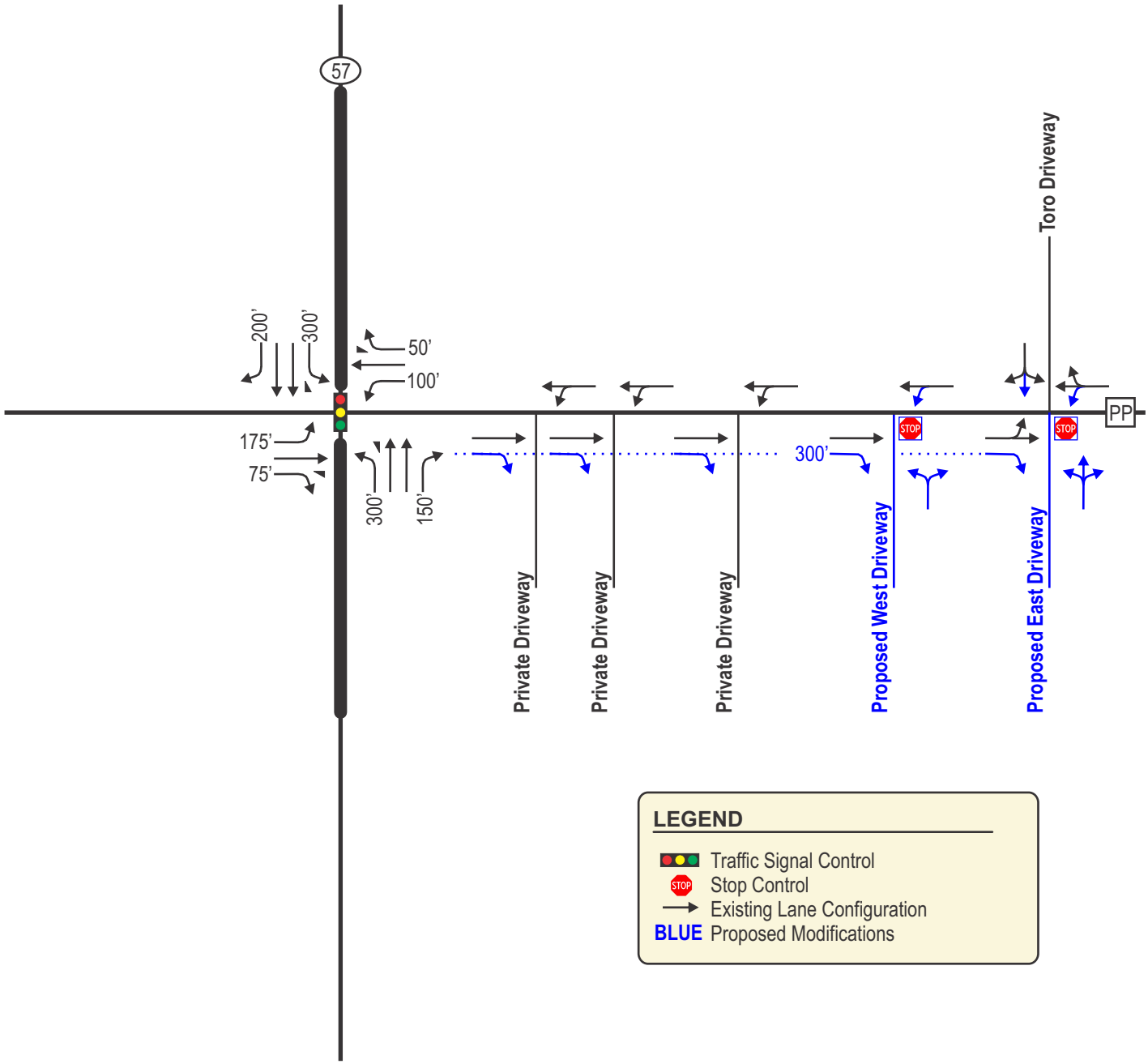

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EXHIBIT 1-3
RECOMMENDED MODIFICATIONS

PLYMOUTH, WISCONSIN

CHAPTER II – PROPOSED DEVELOPMENT

PART A – ON-SITE DEVELOPMENT

A1. Development Description and Site Location

Sargento is proposing to construct a new manufacturing facility to be located on the south side of CTH PP, immediately east of STH 57, in the City of Plymouth. A study area map is shown in Exhibit 2-1.

A2. Land Use and Intensity

A conceptual development site plan is shown in Exhibit 2-2. A 384,000-square-foot manufacturing facility with about 115 employees is proposed to be constructed on the vacant lot. The employee count includes about 25 office/support staff. The facility is expected to operate under three shifts with the first shift from 6:00am to 2:00pm, second shift from 2:00 to 10:00pm and third shift from 10:00pm to 6:00am. According to the owners, about 40 to 50 trucks are expected on the site per day.

A3. Site Plan

As the conceptual site plan shows, access to the Sargento development is proposed via two access driveways along CTH PP. The "East Driveway" is proposed to be located opposite the existing Toro driveway and is expected to be used for employee and visitor access. The "West Driveway" is proposed to be located about 200-feet west of the "East Driveway" and is expected to be used for truck access. Parking for employees and visitors is proposed on the north side of the site and truck docks are proposed on the east and west sides of the site.

A4. Development Phasing and Timing

For the purpose of the TIA, the full build completion of the development was assumed to occur in Year 2026. Therefore, full build out of the Sargento development is included in the Year 2026 Full Build and Year 2036 Full Build traffic scenarios. If the facility expands in the future, it is understood that a future TIA would be required for the expansion plan. A staging diagram is included in Exhibit 2-3.

PART B – STUDY AREA

B1. Influence Area

The primary influence area for this traffic study includes the City of Plymouth and its surrounding communities.

B2. Area of Significant Traffic Impact

The study area includes the following existing intersections, noting that the node number refers to the reference number used in the modeling software.

- Node 100: CTH PP with STH 57 (existing traffic signal control)
- Node 200: CTH PP with Proposed West Driveway (proposed one-way stop control)
- Node 300: CTH PP with Toro Driveway/Proposed East Driveway (existing one-way stop control/proposed two-way stop control)

PART C – OFF-SITE DEVELOPMENT

No off-site developments were identified within the limits of the study area.

PART D – SITE ACCESSIBILITY

D1. Study Area Roadways

The study area roadways are discussed below:

STH 57 is a north/south four-lane divided principal arterial with a posted speed limit of 45 miles per hour (mph) to the north of CTH PP and 55 mph to the south. According to WisDOT, the Year 2024 annual average daily traffic volumes (AADTs) on STH 57 were approximately 13,300 vehicles per day (vpd) immediately north of CTH PP and 10,500-vpd to the south of CTH N. Sidewalks are not currently provided along either side of STH 57 within the limits of the study area.

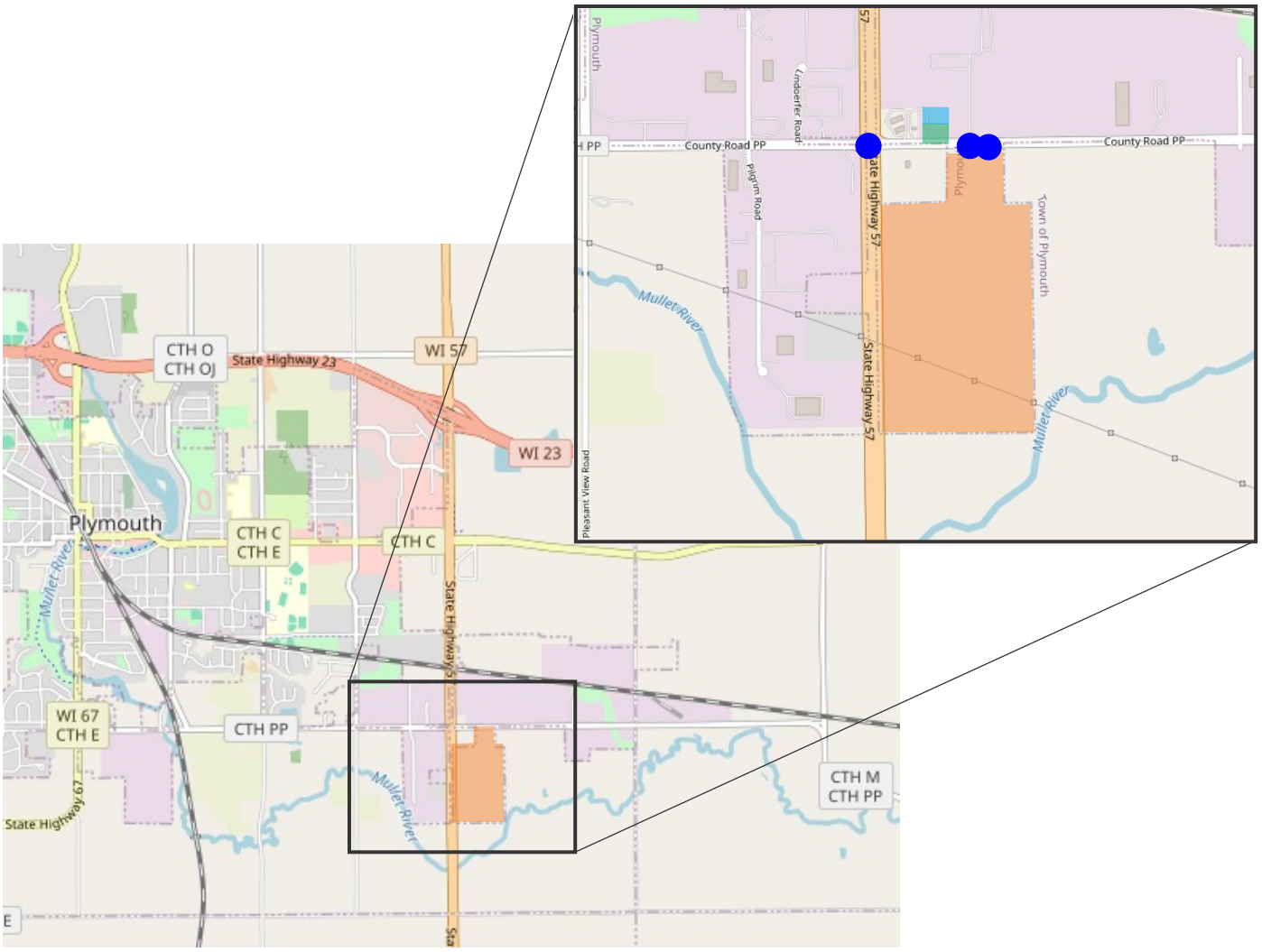
CTH PP is an east/west two-lane undivided minor arterial to the west of STH 57 and a major collector to the east. The posted speed limit on CTH PP is 55 mph. According to WisDOT, the Year 2017 AADT volumes along CTH PP were approximately 6,600-vpd immediately west of STH 57 and 550 (2017 count) to the east, near CTH M. Sidewalks are not currently provided along either side of CTH PP within the limits of the study area.

D2. Pedestrian & Bicycle Accommodations

As described above, no sidewalks or bicycle facilities currently existing within the study area.

D3. Transit Accommodations

Transit is not currently provided in the area.



LEGEND

- Study Area Intersection
- On-Site Development

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EXHIBIT 2-1
PROJECT OVERVIEW MAP

PLYMOUTH, WISCONSIN

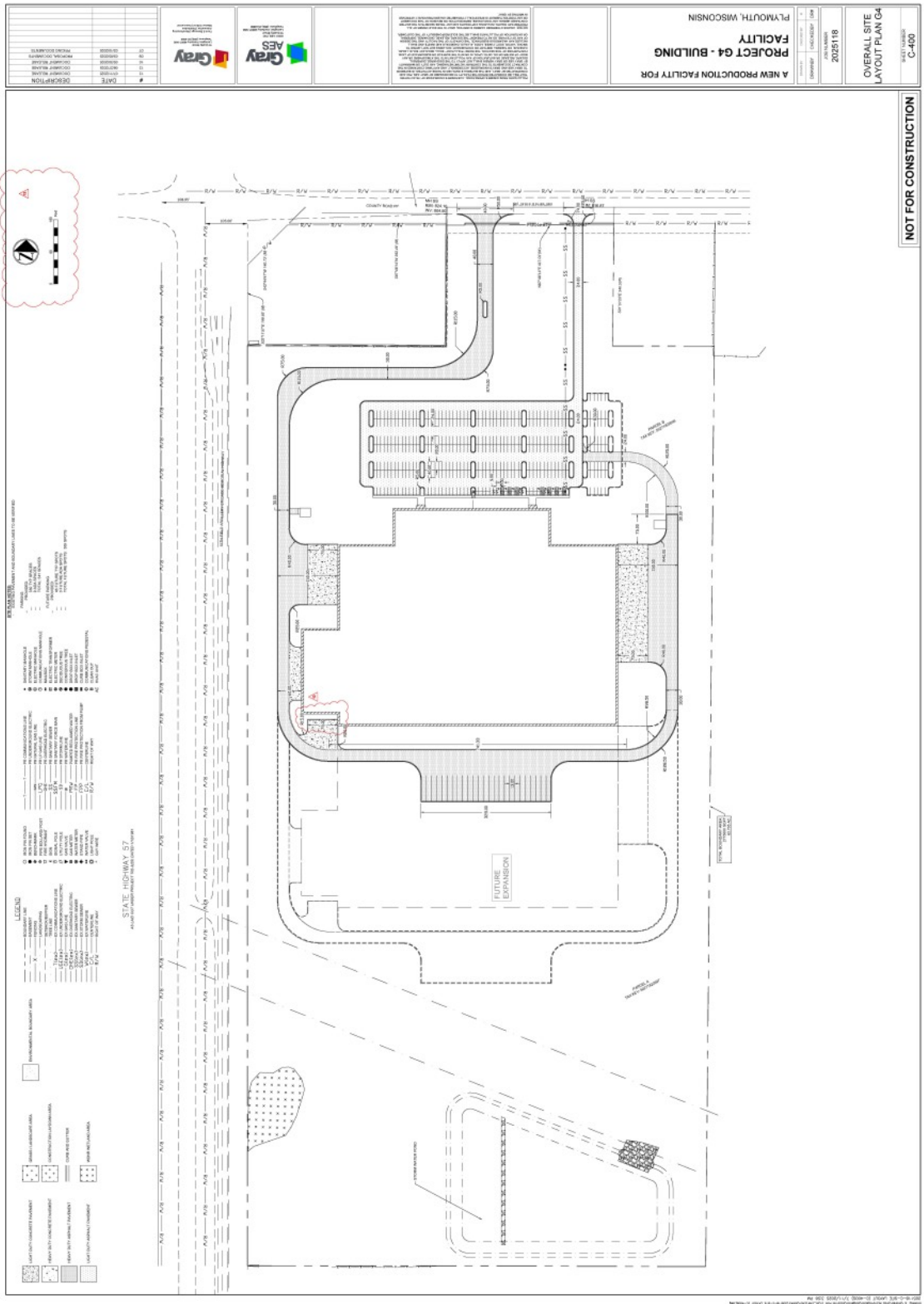

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EXHIBIT 2-2
CONCEPTUAL SITE PLAN

PLYMOUTH, WISCONSIN

CHAPTER III – ANALYSIS OF EXISTING CONDITIONS

PART A – PHYSICAL CHARACTERISTICS

A transportation detail showing existing lane configurations, traffic controls, posted speed limits, and approximate intersection spacing is included in Exhibit 3-1.

PART B – TRAFFIC VOLUMES

The weekday morning and weekday evening peak hours are expected to drive the improvements needed to adequately accommodate the proposed development, as they represent the highest trip generation for the site and the highest volumes along the adjacent roadways. TADI conducted weekday morning (6:00 – 9:00am) and weekday evening (3:00 – 6:00pm) peak hour turning movement traffic counts at the existing study area intersections in mid-June of 2025.

Based on these turning movement counts, the weekday morning and weekday evening peak hours were identified as being 7:00 to 8:00am and 3:00 to 4:00pm, respectively. The Year 2025 Existing traffic volumes are shown in Exhibit 3-2. The traffic counts used to determine peak hour factors and truck percentages have been included in Appendix A.

PART C – CAPACITY LEVEL OF SERVICE ANALYSIS

C1. Level of Service Definitions

The study area intersections were analyzed based on the procedures set forth in the *Highway Capacity Manual, 6th Edition* (HCM). Intersection operation is defined by "level of service." Level of Service (LOS) is a quantitative measure that refers to the overall quality of flow at an intersection ranging from very good, represented by LOS 'A,' to very poor, represented by LOS 'F.' For the purpose of this study, LOS D or better was used to define desirable peak hour operating conditions. Descriptions of the various levels of service are as follows:

Level of Service Definitions

LOS	Signalized Intersections Control Delay/Vehicle (sec/veh)	Unsignalized Intersections Avg. Control Delay (sec/veh)	Relative Delay
A	≤10	≤10	Short Delays
	Free-flow traffic operations at avearge travel speeds. Vehicles completely unimpeded in ability to maneuver. Minimal delay at signalized intersections.		
B	> 10 - 20	> 10 - 15	
	Reasonably unimpeded traffic operations at average travel speeds. Vehicle maneuverability slightly restricted. Low traffic delays.		
C	> 20 - 35	> 15 - 25	
	Stable traffic operations. Lane changes becoming more restricted. Travel speeds reduced to half of average free flow travel speeds. Longer intersection delays.		
D	> 35 - 55	> 25 - 35	Moderate Delays
	Small increases in traffic flow can cause increased delays. Delays likely attributable to increased traffic, reduced signal progression, and adverse timing.		
E	> 55 - 80	> 35 - 50	
	Significant delays. Travel speeds reduced to one-third of average free flow travel speed.		
F	> 80	> 50	Long Delays
	Extremely low speeds. Intersection congestion. Long delays. Extensive traffic queues at intersections.		

Source: Highway Capacity Manual, Transportation Research Board, Washington, D.C., 2010

C2. Year 2025 Existing Traffic Operations

Exhibit 3-3 shows the Year 2025 Existing traffic peak hour operating conditions and expected maximum queues at the study area intersections. The analysis was performed using the Year 2025 Existing traffic volumes (Exhibit 3-2), the existing transportation detail (Exhibit 3-1), and existing traffic signal timings (see Appendix A).

As shown, all movements are currently operating at acceptable LOS C or better at the study area intersections during the typical weekday morning and weekday evening peak periods. Additionally, all maximum traffic queues currently fit within the storage available at all study area intersections.

PART D – SOURCES OF DATA

The following sources of data were obtained for use in conducting this traffic study.

- Turning movement traffic counts – TADI
- AADT Counts – WisDOT
- Existing transportation detail – TADI along with Google Earth
- Traffic signal phasing/timings – WisDOT
- On-site development information – Gray AES

LEGEND

- Traffic Signal Control
- Stop Control
- Existing Lane Configuration
- XX' Distance Between Roadways (in Feet)
- Divided Roadway Median

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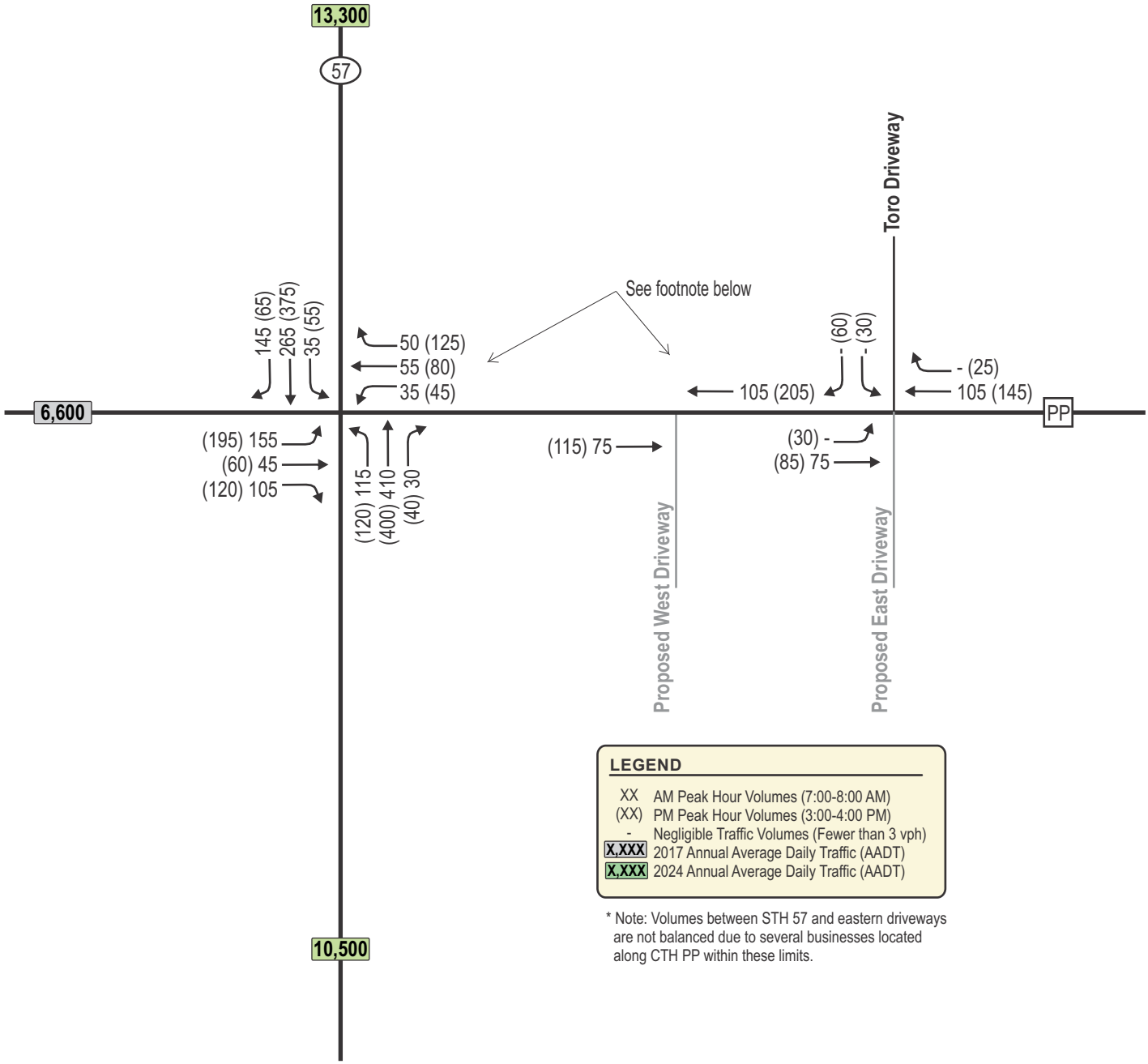
EXHIBIT 3-1
EXISTING TRANSPORTATION DETAIL

PLYMOUTH, WISCONSIN



* Note: Volumes between STH 57 and eastern driveways are not balanced due to several businesses located along CTH PP within these limits.

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EXHIBIT 3-2
YEAR 2025 EXISTING TRAFFIC VOLUMES

PLYMOUTH, WISCONSIN

Exhibit 3-3
Year 2025 Existing Traffic Peak Hour Operating Conditions
With Existing Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach												I/S LOS & Delay
			Eastbound			Westbound			Northbound			Southbound			
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙	
		Lanes->	*1*	*1*	*1*	*1*	*1*	*1*	*1*	*2*	*1*	*1*	*2*	*1*	
Node 100: CTH PP & STH 57	AM	LOS	B	B	B	B	B	B	B	B	B	B	B	B	B
		Delay	18.5	14.7	15.2	16.0	15.2	15.0	14.4	18.6	16.0	15.7	19.8	19.7	17.8
Traffic Signal Control		Queue	110'	35'	50'	30'	45'	30'	55'	125'	25'	25'	85'	75'	
	PM	LOS	C	B	B	B	B	B	B	B	B	B	C	B	B
		Delay	20.1	14.8	15.3	16.4	15.5	16.0	15.1	19.4	16.9	15.3	20.7	18.4	18.4
		Queue	140'	45'	55'	40'	60'	60'	55'	120'	25'	30'	120'	40'	
		Lanes->	*1*		-	-	*1*		-			*1*			
Node 300: CTH PP & Toro Driveway/Propsoed East Driveway	AM	LOS	A		-	-	*		-			A			
		Delay	7.9		-	-	*		-			9.5			
One-Way Stop Control		Queue	25'		-	-	*		-			25'			
	PM	LOS	A		-	-	*		-			A			
		Delay	7.0		-	-	*		-			7.0			
		Queue	25'		-	-	*		-			25'			

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

CHAPTER IV – FORECASTED TRAFFIC

PART A – BACKGROUND TRAFFIC FORECASTING

Historical WisDOT AADT volumes from the year 1990 through year 2024 were collected along STH 57 and CTH PP from the WisDOT historic count station locations adjacent to the subject intersection. Using this data, historical AADT trend lines were developed to determine the expected annual growth of traffic that may occur on the corridor between the present and design year 2036. Based on the traffic projections (1990 – 2024) and assuming the historic trends will continue at a linear rate, traffic volumes along STH 57 are expected to increase at a rate of about 1.5-percent and traffic volumes along CTH PP are expected to increase at a rate of about 2.0-percent. Therefore, these rates were applied to the turning movement volumes collected as part of this study in year 2025 along STH 57 and CTH PP. A 1.5-percent yearly growth was applied to the north/south approaches, and a 2.0-percent yearly growth was applied to the east/west approaches to determine the Year 2036 Background traffic volumes. Growth rate calculations have been included in Appendix A.

The Year 2025 Existing traffic volumes are shown in Exhibit 3-2 in Chapter III. The Year 2036 Background traffic volumes, which include the aforementioned growth rates, are shown in Exhibit 4-1.

PART B – SITE TRAFFIC FORECASTING

To address any potential future traffic impacts along study area roadways and at the intersections adjacent to the development, it is necessary to identify the hourly and daily volume of traffic generated by the proposed development. The traffic volumes expected to be generated by the proposed developments are based on the size and type of the proposed uses, and on expected employee volumes working at the facility. Since the production employee shift changes are expected to take place off peak hour from the adjacent street traffic peak hours, the expected office/support staff counts were used to determine the traffic expected during the adjacent street traffic peak hours. Calculations have been included in Appendix E.

B1. Trip Generation

The Sargento development full build trip generation and distribution are shown in Exhibit 4-2. The proposed Sargento development is expected to generate approximately 30 new trips (25 in/5 out) during the typical weekday morning peak hour, approximately 30 new trips (5 in/25 out) during the typical weekday evening peak hour, and approximately 850 new trips over the course of a typical weekday (425 in/425 out) under full build conditions

B2. Mode Split

Pedestrians, bicyclists, and potential future transit users may utilize their respective modes to access the identified development. However, these modes are expected to make up a very small portion of the overall trips to/from the study area. Therefore, for the purpose of this TIA, all trips to/from the proposed development areas were assumed to occur via motor vehicle.

B3. Determination of Linked and Pass-By Trip Traffic

Linked trips occur when a motorist visits one or more tenants or land uses within a development site. Pass-by trips occur when motorists already on the roadway system stop at a development prior to continuing on their intended route. Due to the proposed land use, it was assumed that linked trips and pass-by trips will be negligible for the proposed development.

B4. Trip Distribution

The trip distribution for the proposed manufacturing facility, which is listed below and shown in table format in Exhibit 4-2 and graphically in Exhibit 4-3, was determined based on the existing traffic counts, the type of proposed land uses and the location of existing populations.

- 45 percent to/from the north on STH 57
- 30 percent to/from the south on STH 57
- 20 percent to/from the west on CTH PP
- 5 percent to/from the east on CTH PP

B5. Trip Assignment

The peak hour new (employees) trips expected to be generated by the full build-out of the proposed Sargento development were assigned to the study area roadways based on the above trip distribution and are shown on Exhibit 4-4A. The peak hour new (truck) trips are shown on Exhibit 4-4B.

PART C – BUILD TRAFFIC VOLUMES

The Year 2026 Full Build traffic volumes, shown in Exhibit 4-5, were determined by summing the Year 2025 Existing traffic volumes (Exhibit 3-2) to the Sargento development new (employee) trips (Exhibit 4-4A) and the Sargento development new (truck) trips (Exhibit 4-4B).

The Year 2036 Full Build traffic volumes, shown in Exhibit 4-6, were determined by summing the Year 2036 Background traffic volumes (Exhibit 4-1) to the Sargento development new trips (Exhibit 4-4A) and the Sargento development new (truck) trips (Exhibit 4-4B).

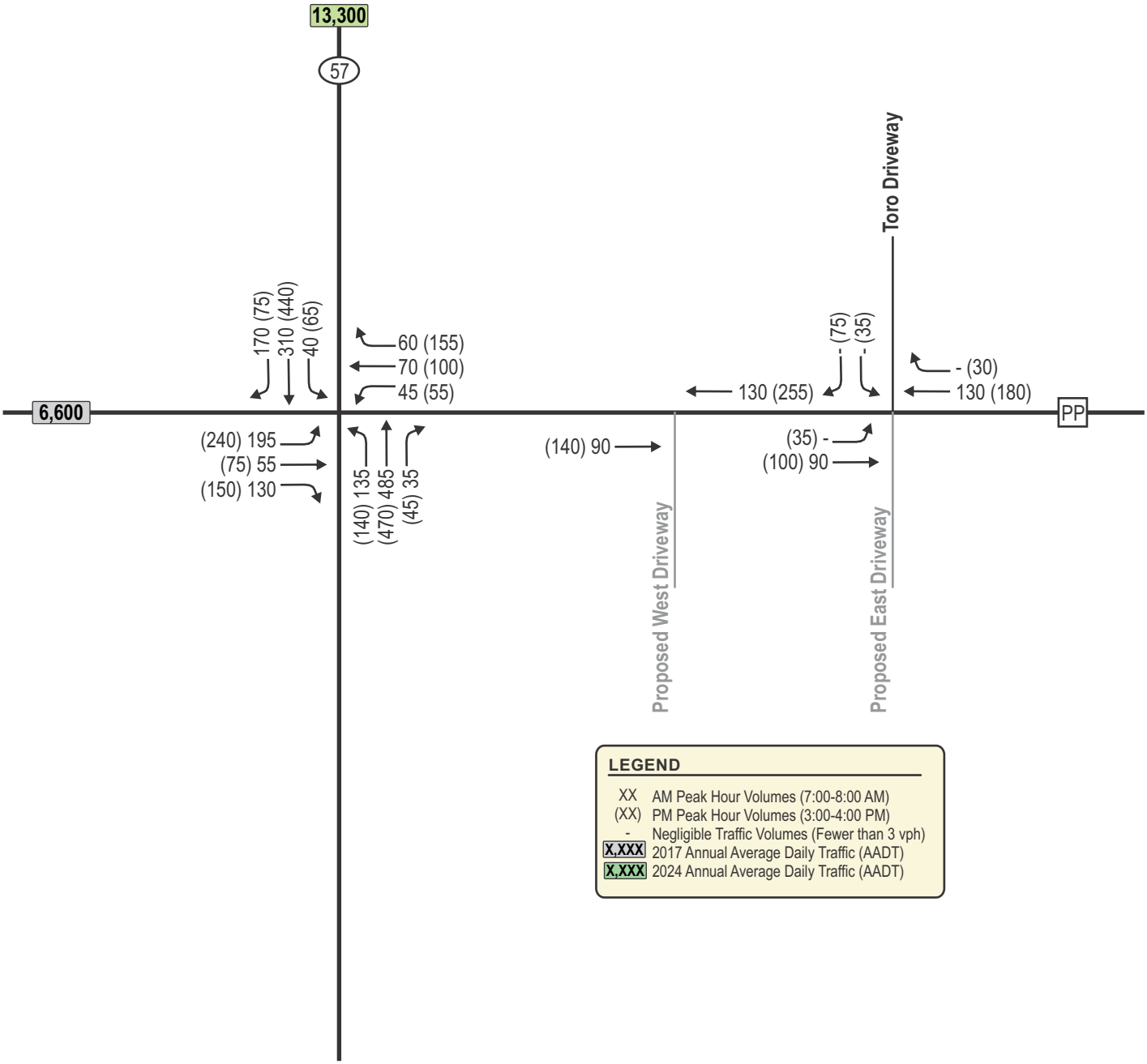

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EXHIBIT 4-1
YEAR 2036 BACKGROUND TRAFFIC VOLUMES

PLYMOUTH, WISCONSIN

Exhibit 4-2

On-Site Trip Generation Table[1]

Land Use	ITE Code	Proposed Size	Weekday Daily	AM Peak In	AM Peak Out	AM Peak Total	PM Peak In	PM Peak Out	PM Peak Total
Manufacturing	TADI	384.000 x 1,000 SF	850	25	5	30	5	25	30
Total New Trips			**850**	**25**	**5**	**30**	**5**	**25**	**30**

[1] *Approx 115 employees per shift (see Appendix E for calculations). Production staff operates under 3 shifts: 6am-2pm, 2pm-10pm, 10pm-6am*
Production staff trips expected off peak. Office/support staff trips expected during peak hour of adjacent street traffic.
Daily trips calculated based on expected employees + expected delivery/truck trips

TRIP DISTRIBUTION (New Trips)

North on STH 57	45%	380	10	5	5	10
South on STH 57	30%	260	10	0	0	10
West on CTH PP	20%	170	5	0	0	5
East on CTH PP	5%	40	0	0	0	0
	100%	***850***	***25***	***5***	***5***	***25***

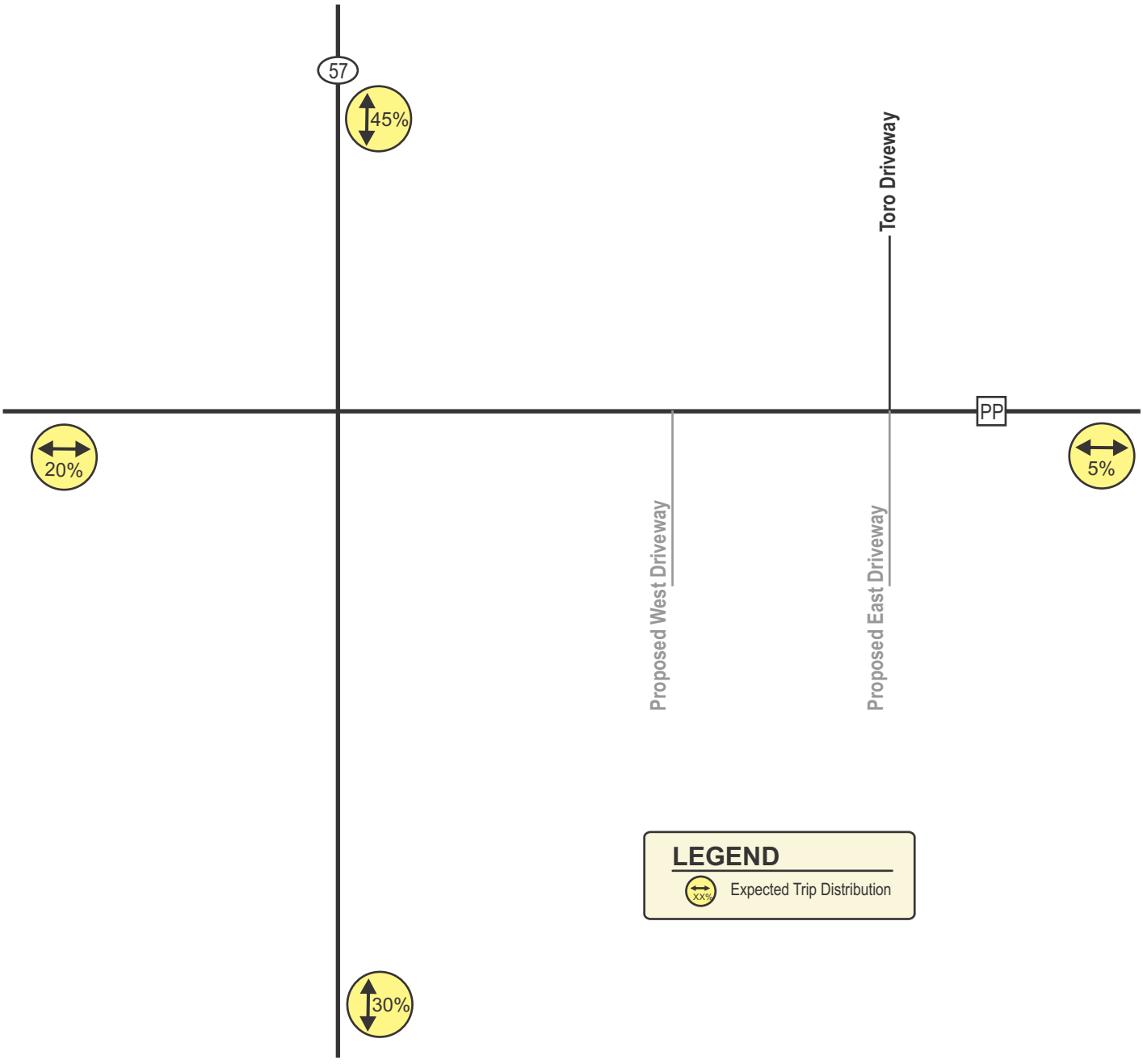

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EXHIBIT 4-3
TRIP DISTRIBUTION DIAGRAM

PLYMOUTH, WISCONSIN

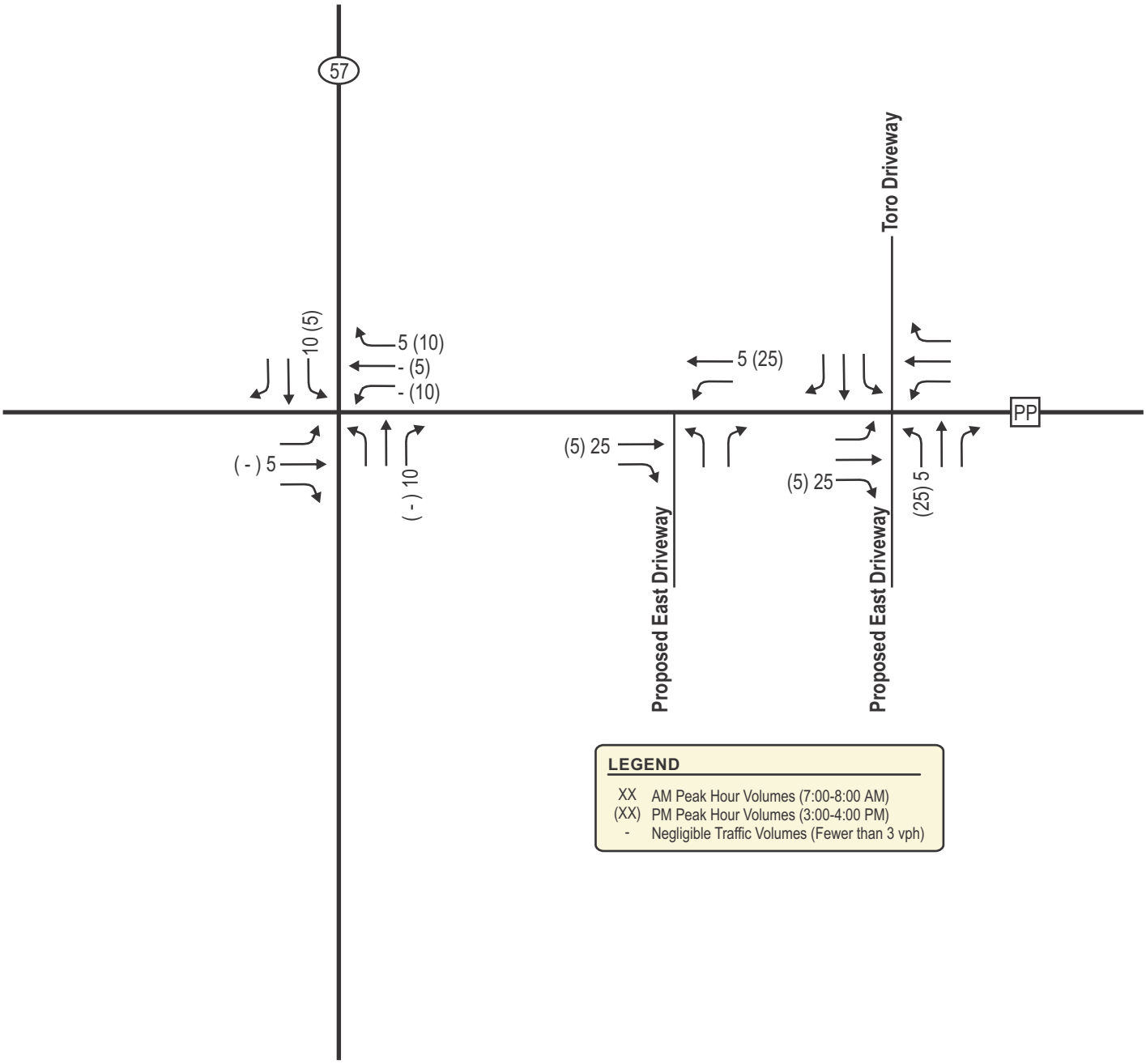

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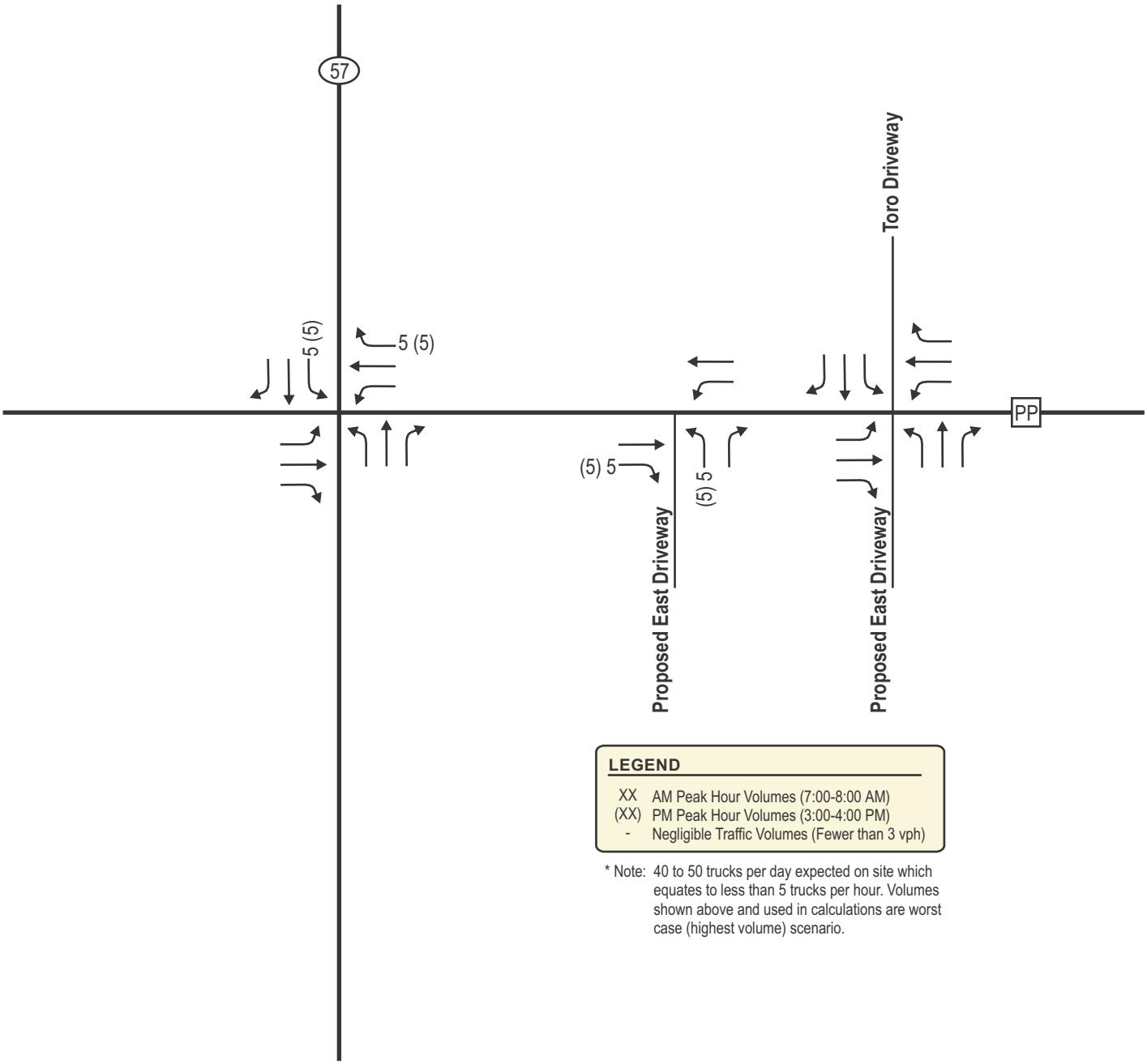
EXHIBIT 4-4A
SARGENTO DEVELOPMENT NEW TRIPS
EMPLOYEE TRIPS

PLYMOUTH, WISCONSIN



LEGEND

XX	AM Peak Hour Volumes (7:00-8:00 AM)
(XX)	PM Peak Hour Volumes (3:00-4:00 PM)
-	Negligible Traffic Volumes (Fewer than 3 vph)

* Note: 40 to 50 trucks per day expected on site which equates to less than 5 trucks per hour. Volumes shown above and used in calculations are worst case (highest volume) scenario.

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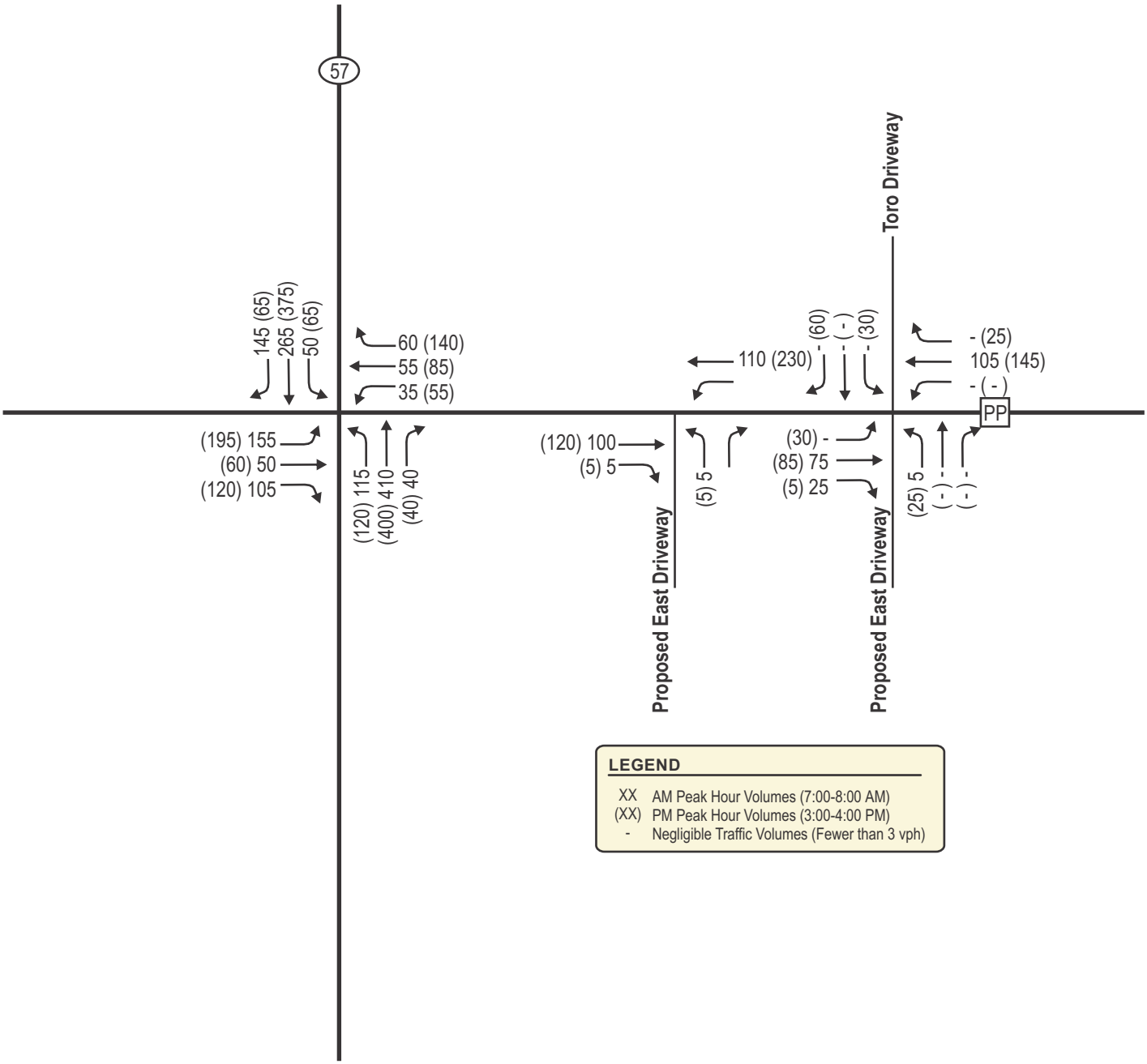
EXHIBIT 4-4B
SARGENTO DEVELOPMENT NEW TRIPS
TRUCK TRIPS

PLYMOUTH, WISCONSIN



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3487 ~ 8/4/2025

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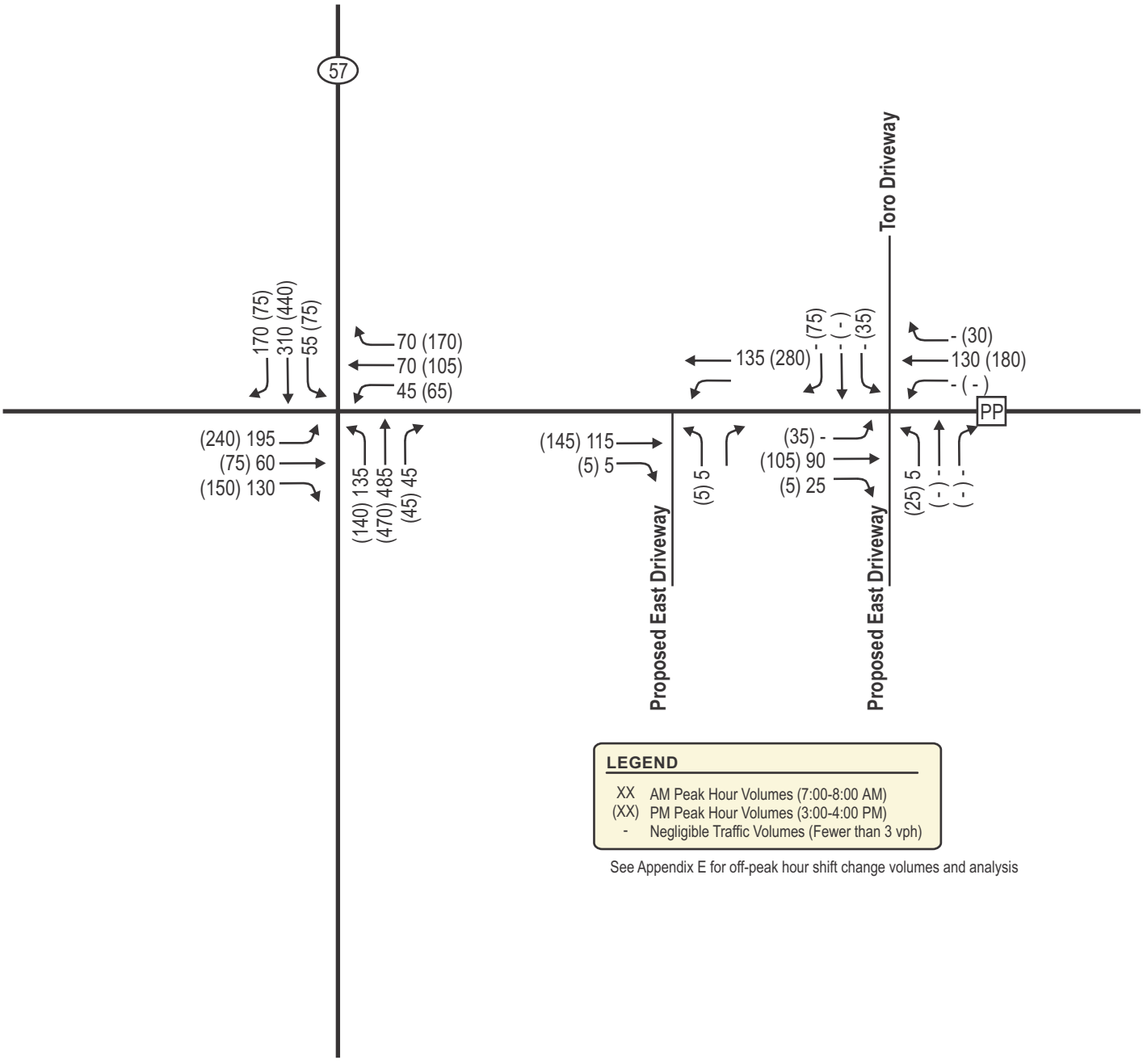
EXHIBIT 4-5
YEAR 2026 FULL BUILD TRAFFIC VOLUMES

PLYMOUTH, WISCONSIN



See Appendix E for off-peak hour shift change volumes and analysis

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NOT TO SCALE

EXHIBIT 4-6
YEAR 2036 FULL BUILD TRAFFIC VOLUMES

PLYMOUTH, WISCONSIN

CHAPTER V – TRAFFIC AND MODIFICATION ANALYSIS

PART A – SITE ACCESS

As the conceptual site plan shows, access to the Sargento development is proposed via two access driveways along CTH PP. The "East Driveway" is proposed to be located opposite the existing Toro driveway and is expected to be used for employee and visitor access. The "West Driveway" is proposed to be located about 200-feet west of the "East Driveway" and is expected to be used for truck access. Parking for employees and visitors is proposed on the north side of the site and truck docks are proposed on the east and west sides of the site.

PART B – CAPACITY LEVEL OF SERVICE ANALYSIS

B1. Traffic Operating Conditions – *No Modifications*

The "no modifications" analyses for Year 2036 Background, Year 2026 Full Build, and Year 2036 Full Build traffic volumes were conducted using the intersection geometrics and controls shown in Exhibit 3-1. All analysis outputs are included in Appendices B through D.

The following outlines the locations of the expected peak hour operating conditions (delay and queueing) without modifications at the study area intersections.

- Exhibit 5-1 – Year 2036 Background Traffic Operations
- Exhibit 5-2 – Year 2026 Full Build Traffic Operations
- Exhibit 5-3 – Year 2036 Full Build Traffic Operations

As shown, all movements are expected to continue to operate at acceptable LOS C or better at the study area intersections during the typical weekday morning and weekday evening peak periods. Additionally, all maximum traffic queues are expected to fit within the available storage at all study area intersections, even under Full Build traffic conditions.

B2. Traffic Operating Conditions – *With Modifications*

Other than the new access driveways, no intersection modifications are recommended for this development. As shown on Exhibits 5-2 & 5-3, all movements are expected to continue to operate at acceptable LOS C or better at the study area intersections during the typical weekday morning and weekday evening peak periods under full build conditions with modifications.

PART C – QUEUEING ANALYSIS

To estimate storage length requirements for turn bays at the study area intersections with modifications, a queuing analysis has been conducted. Note that the 95th percentile probable queue lengths were used for the design of turn bay storage at controlled intersections. The following is a list of where the results of the queuing analysis can be found.

- Year 2025 Existing Traffic – Exhibit 3-3 & 5-4
- Year 2026 Background Traffic – Exhibit 5-1 & 5-5
- Year 2026 Full Build Traffic – Exhibit 5-2 & 5-6
- Year 2036 Full Build Traffic – Exhibit 5-3 & 5-7

PART D – TURN LANE WARRANT ANALYSIS

Left-turn Lane Analysis - Facilities Development Manual

FDM Section 11-25-5, Table 5.2, provides guidance on warranting left-turn lanes at intersections on two-lane highways. Based on the volume criteria provided and using a design speed of 5-mph over the posted speed, or 60-mph, a dedicated westbound left-turn lane is not expected to be warranted at the proposed access drive along CTH PP under year 2036 full

build traffic volume conditions, even during the highest volume shift changes as shown in the sensitivity analysis traffic volumes. Trip generation, volume exhibits and calculation spreadsheets/tables utilizing both sensitivity analysis traffic volume scenarios are provided in the appendix of this report.

Right-turn Lane Analysis - NCHRP Report 457

As referenced in the FDM, NCHRP Report 457 provides guidance for inclusion of a right-turn lane on a high-speed roadway based on the expected peak hour right-turn volume in relation to the major road peak hour through volume as well as the 85th percentile speed limit. Since the posted speed limit on CTH PP at the proposed roadways is 55-mph, a 60-mph speed was assumed for the 85th percentile speed. Utilizing t both sensitivity analysis traffic volume scenarios and as shown in the traffic volumes and corresponding graph in the appendix, for the advancing eastbound shift change peak hour volume of either 210 or 225 and a right-turn volume of 85 (projected) for the typical weekday evening peak hour during the highest volume shift changes, the right-turn warrant is expected to be met. Trip generation, volume exhibits and calculation spreadsheets/tables are provided in the appendix of this report.

Exhibit 5-1
Year 2036 Background Traffic Peak Hour Operating Conditions
With Existing Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach												I/S LOS & Delay
			Eastbound			Westbound			Northbound			Southbound			
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙	
		Lanes->	*1*	*1*	*1*	*1*	*1*	*1*	*1*	*2*	*1*	*1*	*2*	*1*	
Node 100: CTH PP & STH 57	AM	LOS	C	B	B	B	B	B	B	B	B	B	C	C	B
		Delay	20.3	14.9	15.6	16.7	15.7	15.3	14.8	19.4	16.2	15.8	20.3	20.2	18.5
Traffic Signal Control		Queue	140'	40'	60'	40'	50'	35'	65'	145'	25'	25'	100'	85'	
	PM	LOS	C	B	B	B	B	B	B	C	B	B	C	B	B
		Delay	23.2	15.1	15.7	17.2	15.9	16.6	15.6	20.4	17.2	15.4	21.6	18.6	19.4
		Queue	190'	60'	70'	45'	75'	75'	65'	140'	30'	35'	145'	40'	
		Lanes->	*1*		-	-	*1*		-			*1*			
Node 300: CTH PP & Toro Driveway/Propsoed East Driveway	AM	LOS	A		-	-	*		-			A			
		Delay	8.0		-	-	*		-			9.8			
One-Way Stop Control		Queue	25'		-	-	*		-			25'			
	PM	LOS	A		-	-	*		-			B			
		Delay	8.3		-	-	*		-			12.3			
		Queue	25'		-	-	*		-			25'			

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

Exhibit 5-2
Year 2026 Full Build Traffic Peak Hour Operating Conditions
With Existing Geometrics and Traffic Control

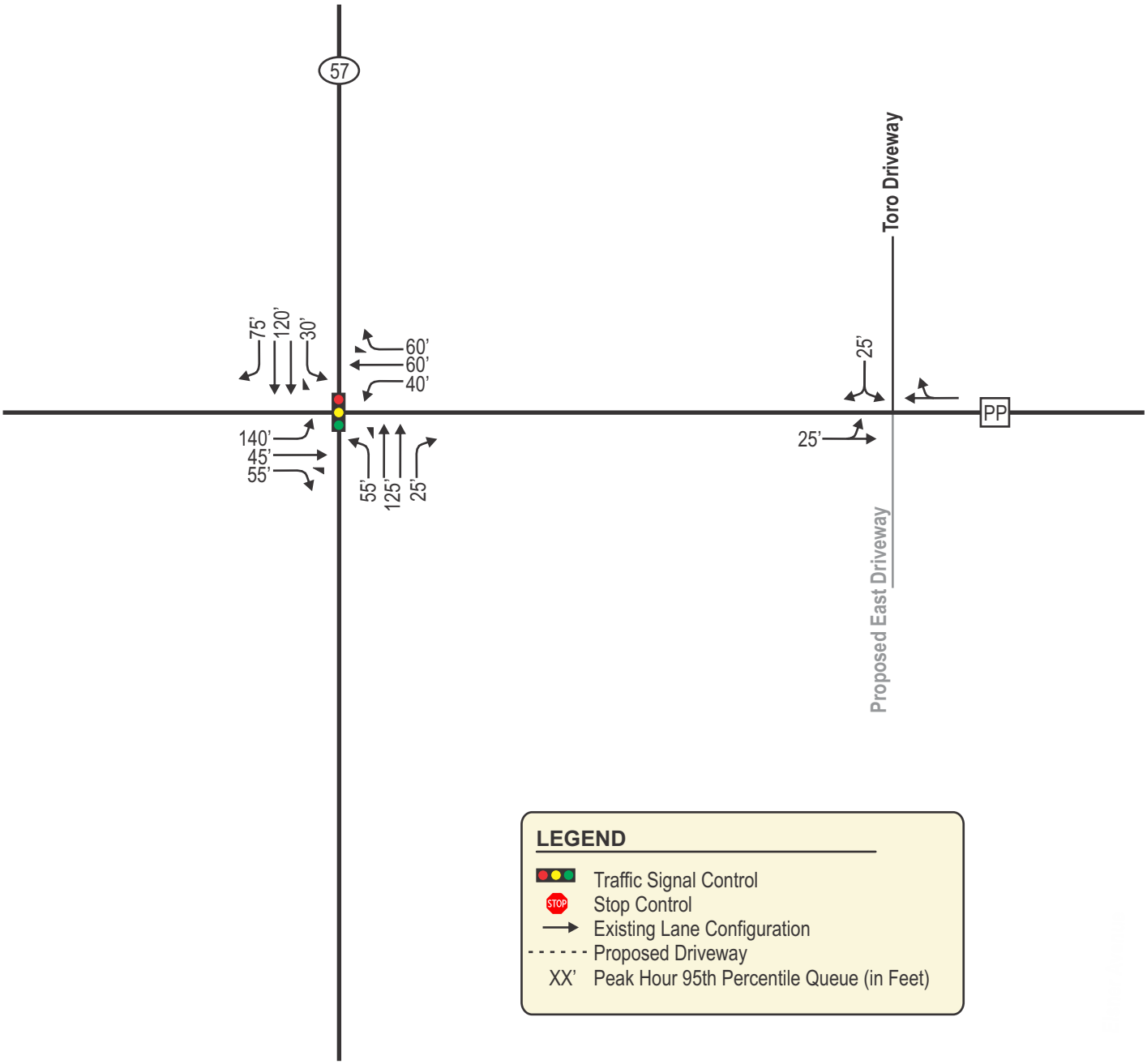
Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach												I/S LOS & Delay
			Eastbound			Westbound			Northbound			Southbound			
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙	
		Lanes->	*1*	*1*	*1*	*1*	*1*	*1*	*1*	*2*	*1*	*1*	*2*	*1*	
Node 100: CTH PP & STH 57	AM	LOS	B	B	B	B	B	B	B	B	B	B	B	B	B
		Delay	18.5	14.7	15.2	16.1	15.2	15.2	14.8	19.3	16.7	15.4	19.8	19.7	18.0
Traffic Signal Control		Queue	110'	40'	50'	30'	45'	35'	55'	125'	25'	30'	85'	75'	
	PM	LOS	C	B	B	B	B	B	B	B	B	B	C	B	B
		Delay	20.3	14.8	15.3	16.7	15.5	16.2	15.1	19.7	17.2	15.1	20.7	18.4	18.5
		Queue	140'	45'	55'	45'	60'	65'	55'	120'	25'	35'	120'	40'	
Node 200: CTH PP & Propsoed West Driveway		*Lanes->*	-	*1*		*1*		-	*1*			-			
	AM	LOS	-	*		A		-	B			-			
One-Way Stop Control		Delay	-	*		7.8		-	11.3			-			
		Queue	-	*		25'		-	25'			-			
	PM	LOS	-	*		A		-	B			-			
		Delay	-	*		7.7		-	13.2			-			
		Queue	-	*		25'		-	25'			-			
Node 300: CTH PP & Toro Driveway/Propsoed East Driveway		*Lanes->*	*1*			*1*			*1*			*1*			
	AM	LOS	A			A			B			B			
Two-Way Stop Control		Delay	7.9			7.8			10.3			10.1			
		Queue	25'			25'			25'			25'			
	PM	LOS	A			A			B			B			
		Delay	8.1			7.6			13.3			11.6			
		Queue	25'			25'			25'			25'			

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

Exhibit 5-3
Year 2036 Full Build Traffic Peak Hour Operating Conditions
With Existing Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach												I/S LOS & Delay
			Eastbound			Westbound			Northbound			Southbound			
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙	
		Lanes->	*1*	*1*	*1*	*1*	*1*	*1*	*1*	*2*	*1*	*1*	*2*	*1*	
Node 100: CTH PP & STH 57	AM	LOS	C	B	B	B	B	B	B	C	B	B	C	C	B
		Delay	20.3	15.0	15.6	16.8	15.7	15.5	15.1	20.2	16.9	15.6	20.3	20.2	18.7
Traffic Signal Control		Queue	140'	45'	60'	40'	55'	35'	60'	150'	30'	30'	100'	85'	
	PM	LOS	C	B	B	B	B	B	B	C	B	B	C	B	B
		Delay	23.6	15.1	15.7	17.5	16.0	16.9	15.6	20.7	17.4	15.4	21.6	18.6	19.5
		Queue	190'	60'	70'	55'	75'	80'	65'	145'	30'	40'	145'	40'	
Node 200: CTH PP & Propsoed West Driveway		*Lanes->*	-	*1*		*1*		-	*1*			-			
	AM	LOS	-	*		A		-	B			-			
		Delay	-	*		7.8		-	11.8			-			
One-Way Stop Control		Queue	-	*		25'		-	25'			-			
	PM	LOS	-	*		A		-	B			-			
		Delay	-	*		7.8		-	14.5			-			
		Queue	-	*		25'		-	25'			-			
Node 300: CTH PP & Toro Driveway/Propsoed East Driveway		*Lanes->*	*1*			*1*			*1*			*1*			
	AM	LOS	A			A			B			B			
		Delay	8.0			7.8			10.8			10.5			
Two-Way Stop Control		Queue	25'			25'			25'			25'			
	PM	LOS	A			A			C			B			
		Delay	8.3			7.7			15.2			13.0			
		Queue	25'			25'			25'			25'			

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.



TRAFFIC ANALYSIS & DESIGN, INC.

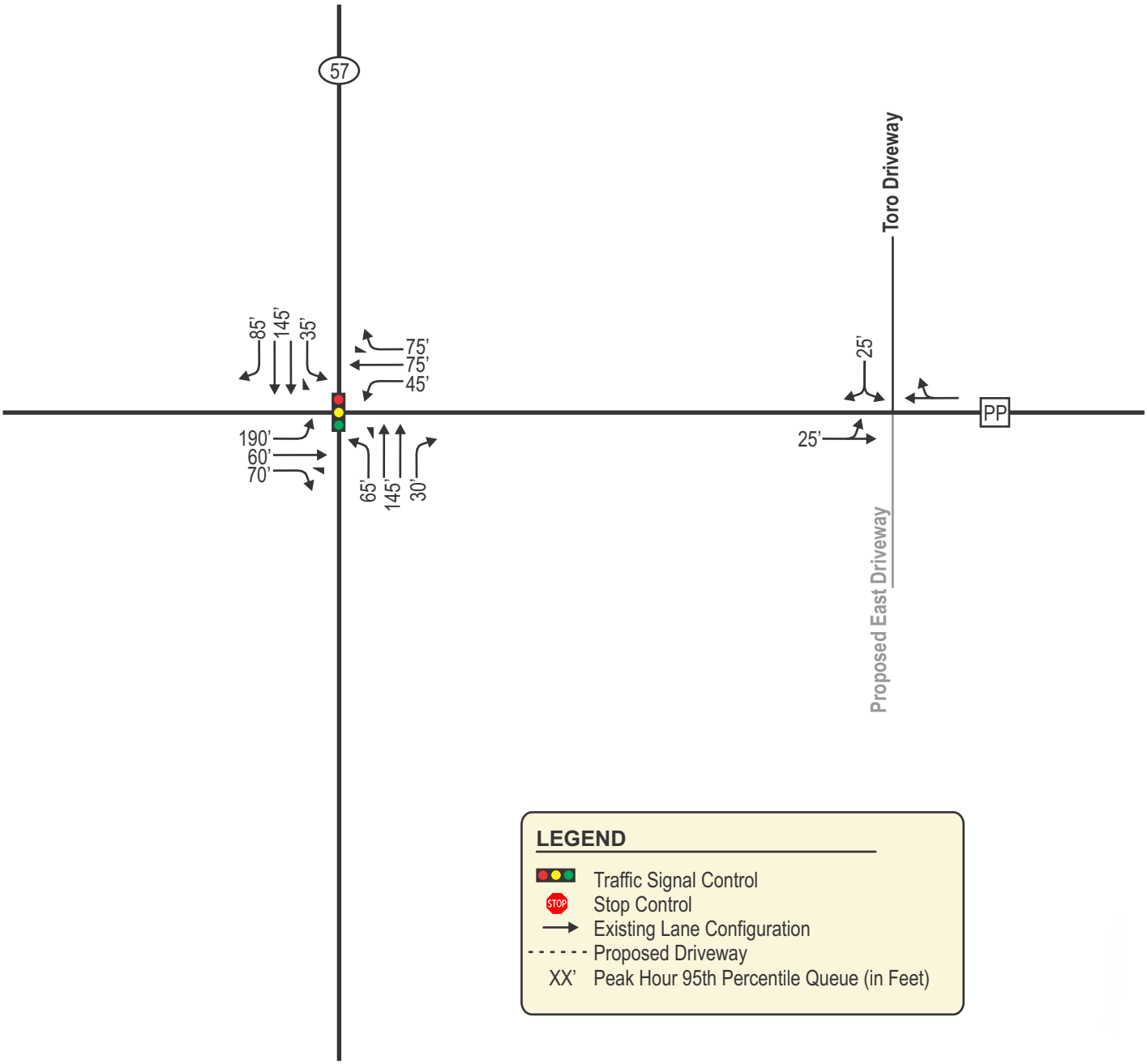
3487 ~ 8/4/2025

N

NOT TO SCALE

EXHIBIT 5-18
YEAR 2025 EXISTING TRAFFIC VOLUMES
MAXIMUM QUEUE LENGTHS

PLYMOUTH, WISCONSIN



TRAFFIC ANALYSIS & DESIGN, INC.

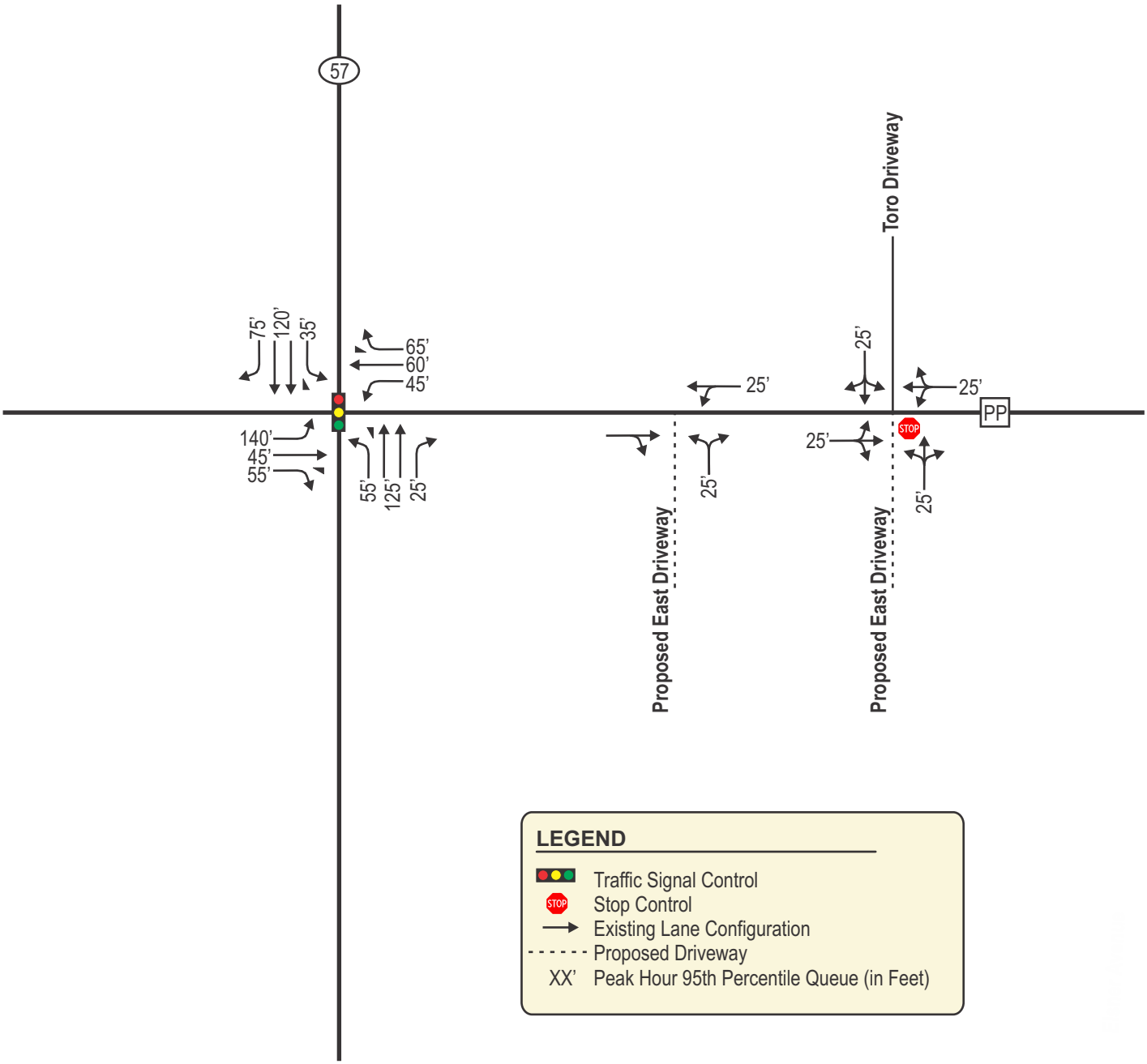
3487 ~ 8/4/2025

N

NOT TO SCALE

EXHIBIT 5-19
YEAR 2036 BACKGROUND TRAFFIC VOLUMES
MAXIMUM QUEUE LENGTHS

PLYMOUTH, WISCONSIN

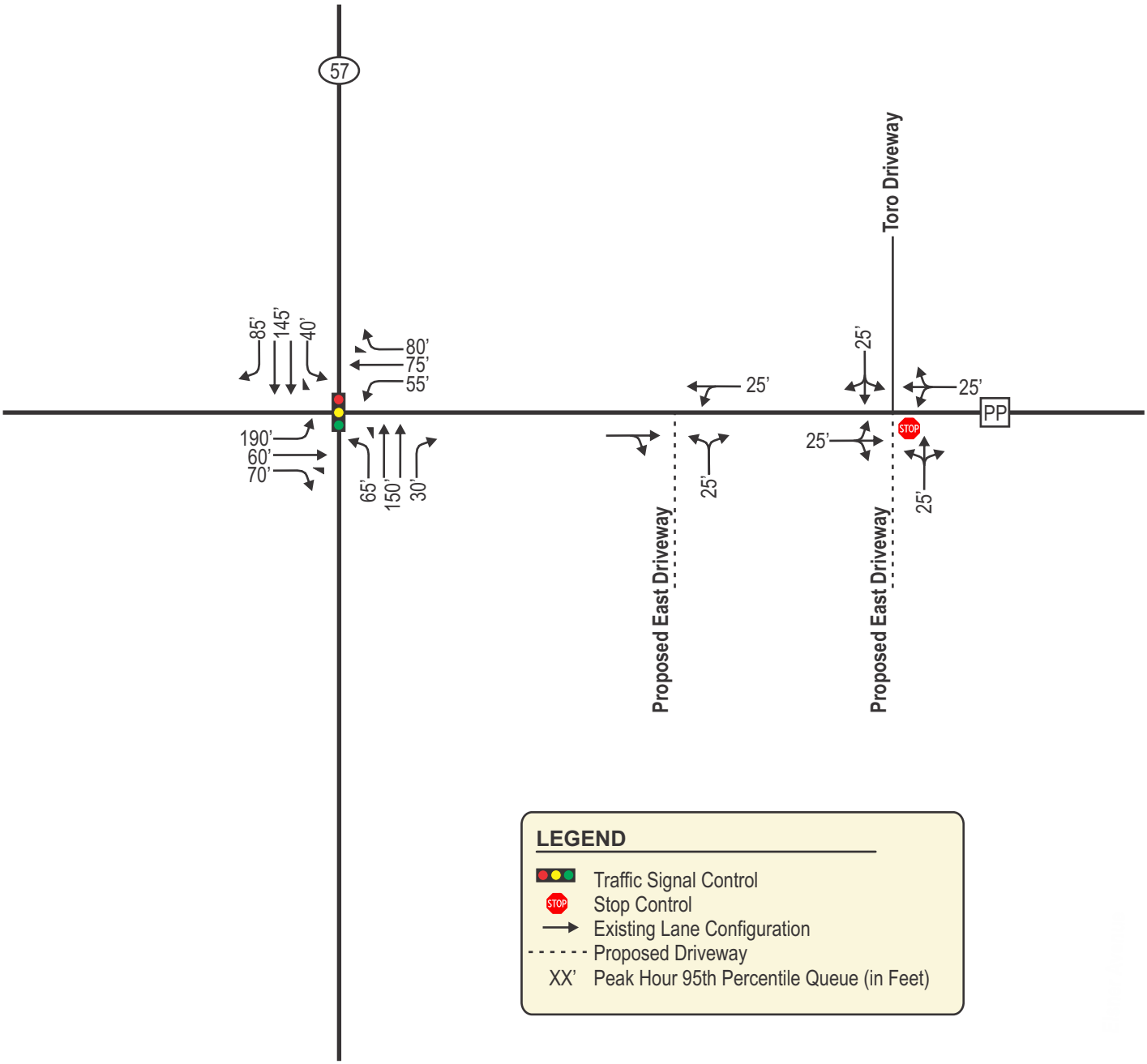


TRAFFIC ANALYSIS & DESIGN, INC.
3487 ~ 8/4/2025

N
NOT TO SCALE

EXHIBIT 5-20
YEAR 2026 BUILD TRAFFIC VOLUMES
MAXIMUM QUEUE LENGTHS

PLYMOUTH, WISCONSIN



TRAFFIC ANALYSIS & DESIGN, INC.
3487 ~ 8/4/2025

N
NOT TO SCALE

EXHIBIT 5-21
YEAR 2036 BUILD TRAFFIC VOLUMES
MAXIMUM QUEUE LENGTHS

PLYMOUTH, WISCONSIN

CHAPTER VI – RECOMMENDATIONS AND CONCLUSION

PART A – RECOMMENDED MODIFICATIONS

The study area intersections were analyzed based on the procedures set forth in the *Highway Capacity Manual, 6th Edition* (HCM). Intersection operation is defined by "level of service." Level of Service (LOS) is a quantitative measure that refers to the overall quality of flow at an intersection ranging from very good, represented by LOS 'A,' to very poor, represented by LOS 'F.' For the purpose of this study, LOS D or better was used to define desirable peak hour operating conditions.

A1. Recommended Modifications

Recommended modifications, shown in Exhibit 1-3, are split into the following categories:

- "Existing Traffic" – These modifications are expected to be necessary to accommodate the Year 2025 Existing traffic volumes without the proposed development.
- "Background Traffic" – These modifications are expected to be necessary to accommodate the Year 2036 Background traffic without the proposed development, which includes the modifications required from general background growth in the study area through the year 2036. The background traffic recommended modifications are in addition to conditions as they currently exist.
- "Full Build Traffic" – These modifications are expected to be necessary to accommodate the Year 2036 Full Build traffic volumes which includes full build out of the proposed Sargento development as well as background traffic growth in the study area through the year 2036. Full Build traffic modifications are in addition to background traffic recommended modifications. Note that the modifications recommended are for both the year 2026 and the year 2036.

The analysis was conducted using existing intersection geometrics and traffic control and the existing traffic signal timings. The following modifications, as shown in Exhibit 1-3, are recommended to accommodate the Year 2025 Existing, Year 2036 Background and Year 2036 Full Build traffic volumes, respectively. *Modifications are for jurisdictional consideration and are not legally binding. The City of Plymouth and Sheboygan County reserve the right to determine alternative solutions.*

Node 100: CTH PP & STH 57

- *Existing Traffic:* No modifications.
- *Background Traffic:* No modifications.
- *Full Build Traffic:* No modifications.

Node 200: CTH PP & Proposed West Driveway

- *Existing Traffic:* No modifications.
- *Background Traffic:* No modifications.
- *Full Build Traffic:*
  - Provide a single lane driveway on the south approach as shown on the conceptual site plan.
  - Construct a dedicated right-turn lane on the west approach (distance shown on Exhibit 1-3).

- Provide stop sign control on the south approach.

Node 300: CTH PP & Toro Driveway/Proposed East Driveway

- *Existing Traffic:* No modifications.
- *Background Traffic:* No modifications.
- *Full Build Traffic:*
  - Provide a single lane driveway on the south approach as shown on the conceptual site plan.
  - Construct a dedicated right-turn lane on the west approach (construct up to and through the west driveway).
  - Provide stop sign control on the south approach.

In addition to the typical weekday morning and weekday afternoon commuter peak hour analysis, a separate afternoon shift change off peak hour sensitivity analysis was also completed for the study area intersections (calculations and exhibits shown in Appendix E). Due to the shift changes occurring during the non-peak hours on CTH PP and STH 57, when traffic volumes on CTH PP and STH 57 are lower, the separate sensitivity analysis was conducted during the 1:30 to 2:30pm hour, where the 1st shift employees are leaving and the 2nd shift employees are entering. Based on a review of the historic WisDOT hourly traffic volumes on CTH PP and STH 57, the 1:00 to 2:00pm and 2:00 to 3:00pm traffic volumes are approximately 15- to 10-percent lower than the 3:00 to 4:00pm peak hour traffic volumes, respectively. Therefore, the Year 2036 3:00 to 4:00pm existing/background peak hour traffic volumes were reduced by 10-percent to represent the 1:30 to 2:30pm shift change hour. During this hour and with the shift change, 90 vehicles are expected to enter the proposed Sargento facility, and 90 vehicles are expected to exit, using the same percentage distribution as described later in this report. This additional nonpeak hour shift change traffic was added to the adjusted 1:30 to 2:30pm adjusted traffic turning movements to determine the total traffic volumes. This sensitivity analysis traffic was analyzed for the Year 2036 full build (1:30 shift change) conditions which resulted in LOS C or better for all turning movements and all queues expected to fall within the existing turn lane and painted taper lengths. It is noted that the overall sensitivity analysis traffic volumes with the shift change of 1:30 to 2:30pm are less than the Year 2036 full build traffic volumes from 3:00 to 4:00pm peak hour traffic. The sensitivity analysis traffic shift change off peak hour (1:30 to 2:30pm) Year 2036 Build traffic volumes at the CTH PP driveway were utilized to determine if left or right turn lanes are warranted on CTH PP.

A separate afternoon sensitivity analysis was also completed for the study area intersections assuming the afternoon shift change would occur during the typical peak hour of adjacent street traffic, or 3:00 to 4:00pm (calculations and exhibits shown in Appendix F). During this hour and with the shift change, 90 vehicles are expected to enter the proposed Sargento facility, and 90 vehicles are expected to exit, as described in the previous paragraph. This additional peak hour shift change traffic was added to the 3:00 to 4:00pm peak hour traffic turning movements to determine the total traffic volumes. This sensitivity analysis traffic was analyzed for the Year 2036 full build (3:00pm shift change) conditions which resulted in LOS C or better for all turning movements and all queues expected to fall within the existing turn lane and painted taper lengths. The sensitivity analysis traffic shift change peak hour (3:00 to 4:00pm) Year 2036 Build

traffic volumes at the CTH PP driveway were also utilized to determine if left or right turn lanes are warranted on CTH PP.

The proposed development is not expected to significantly impact traffic operations at the study intersections. Delay and queue increases are expected to be minimal with no changes in Levels of Service. As described above, the eastbound right-turning movement volumes on CTH PP at the development access driveway are high enough to warrant a dedicated right-turn lane on CTH PP at the east driveway. A dedicated right-turn lane at the west driveway is also recommended due to truck traffic exiting the high speed CTH PP facility. The westbound left-turning movement volumes are not high enough to warrant a left-turn lane on CTH PP.

PART B – CONCLUSION

The study area intersections are expected to operate desirably at LOS C or better conditions with the anticipated development and with the identified recommended modifications if properly designed and implemented through the design year 2036.

7711 N. Port Washington Road
Milwaukee, WI 53217
T: 414.751.7200 • F: 414.351.4117

Ms. Cathy Austin, P.E.
City of Plymouth
900 CTH PP – P.O. Box 277
Plymouth, WI, 53073

28 August 2025

From: Neal Styka, P.E., PTOE, RSP1 - Kapur & Associates, Inc.
CC: Cathy Austin, P.E. – City of Plymouth
Aaron Groh–Public Works - Kapur & Associates, Inc.

RE: Sargento Development Traffic Impact Analysis (TIA) Review

Dear Cathy:

Kapur has reviewed the traffic impact analysis for Sargento's new manufacturing facility to be located on the south side of CTH PP immediately east of STH 57, in the City of Plymouth, Sheboygan County, Wisconsin.

The TIA was reviewed based on the Institute of Transportation Engineers (ITE) Trip Generation Manual, 11th Edition and the Wisconsin Department of Transportation (WisDOT) Traffic Impact Analysis Guidelines most recent update of April 2024.

Documents Submitted:
Traffic Impact Analysis and Attachments dated July 3rd, 2025, and its resubmittal dated August 4th (received August 15th), and a revision to Appendix F provide on August 27th, 2025.

Comments
All of the TIA comments contained within our initial review email have been satisfactorily addressed.

Conclusion
The study intersections included in this TIA:

- CTH PP with STH 57 (existing traffic signal control)
- CTH PP with Proposed West Driveway (proposed one-way stop control)
- CTH PP with Toro Driveway/Proposed East Driveway (existing one-way stop control/proposed two-way stop control)

The study periods are the weekday morning and weekday evening peak hours, 7:00 to 8:00am and 3:00 to 4:00pm, respectively. A sensitivity analysis was performed to determine operations at the afternoon shift change. Currently it is proposed to occur at 2:00 pm so an analysis of 1:30 to 2:30 pm was conducted when the greatest number of employees would be entering and exiting the site. Additionally, another sensitivity analysis was conducted for the 3:00 to 4:00 pm peak hour to determine operations if the shift change time ever moved and overlapped with the PM Peak Hour.
Per the TIA, "The proposed development is not expected to significantly impact traffic operations at the study intersections. Delay and queue increases are expected to be minimal with no changes in Levels of Service." A dedicated right turn lane on eastbound CTH PP for the east driveway is warranted. A left turn lane for westbound traffic is not warranted as the volumes were not high enough.
The storage length for the turn lanes at the intersection of CTH PP and STH 57 are adequate for all movements with the estimated 95th percentile queue, except for the eastbound left turn from CTH PP to STH 57 and westbound right turn from CTH PP to STH 57. In the 2036 background and build scenarios, the eastbound left turn from CTH PP to STH 57 is approximately 15' short (<1 car length) and 20' short in the 3:00 pm shift change scenario. The storage length for the westbound right turn from CTH PP to STH 57 is 10' short of the expected maximum queue with 2025 existing traffic and is expected to increase to 30' short 2036 with build traffic.

Please call (414) 751-7238 or email me at nstyka@kapurinc.com if you have any questions or comments regarding this review.

Sincerely,
Kapur

Neal Styka, P.E., PTOE, RSP1

City of Plymouth
128 Smith St. - P.O. Box 107
Plymouth, WI 53073-0107

Telephone: **(920) 893-3745**
Facsimile: **(920) 893-0183**
Web Site: **plymouthgov.com**

DATE: August 26, 2025

TO: Plan Commission

FROM: Jack Johnston, Assistant City Administrator/Community Development Director

RE: **Concept Plan Seeking Feedback;** Parcel Number 59271829210, located on Section 33 along State Highway 67 on the southwest side of the City of Plymouth, regarding a concept plan for a single-family residential subdivision plat and multi-family residential project. Neumann Developments, Inc. (enclosure)

Background:

Neumann Development, Inc. is coming before the Plan Commission to seek general feedback on concept plan for a subdivision project located on the State Highway 67 curve just south of the Greystone subdivision on a ~67-acre vacant parcel currently owned by the Plymouth Industrial Development Corporation (PIDC). This is a concept plan only and not a formal application. The concept plan shows 119 single family lots along with 60 multi-family residential units on the southern end of the site. Neumann would serve as developer for the single-family portion of the project while Premier Real Estate (PRE/3) would serve as developer for the multi-family portion of the project. The concept plan also shows a new park to be located within the development to be used by area residents. The development would be serviced by roads connecting to both STH 67 and the Greystone subdivision to the north.

Comprehensive Plan/Zoning Summary/Setbacks:

The property's current zoning and designation on the future land use map on the 2022 Comprehensive Plan are both agricultural. As such, a comprehensive plan amendment application and rezoning petition will be required during the City application process to ensure harmony with both the Comprehensive Plan and City's zoning map. The applicant would need to seek a comprehensive plan amendment to change the designation of land from agricultural to residential in order to seek a future rezone of the property into one of the various residential zoning districts. The applicant cannot simply seek a rezone into a residential district without the comprehensive plan allowing residential on the property.

The applicant's concept plan has shown an indication for preference of single-family residential lot sizes of 8,700 square feet minimum, lot widths of 70' minimum, front yard setbacks of 25', front side corner setbacks of 15', and 7.5' of side yard setback. The zoning matrix found on the next page shows these requests do not adhere to the R-2 zoning district, which is the most popular single family residential district in the City and is used for the Greystone and South Hills subdivision to the north. It does however adhere to the City's R-5 district which is the underlying zoning district in the Vintage Neighborhood off of Pleasant View Road.

Single-Family Residential Zoning:

ZONING CONSISTENCY MATRIX			
	Provided	***Required***	***Required***
Zoning	Neumann Concept Plan	R-2	R-5
Minimum Lot Size	8,700 sq ft	10,000 sq ft	6,000 sq ft
Minimum Frontage	70’	60’	None
Maximum Lot Coverage of All Buildings	Not Shown	50%	90%
Minimum Lot Width at Building Line	70’	80’	60’ or 45’ for irregular shaped lot
Minimum Front Yard Setback	25’	30’	8’
Minimum Side Yard Setback	7.5’	9’	Minimum 5’ from either side, total of at least 12’

Staff is generally comfortable with the lot sizes and setbacks proposed by Neumann as they do meet the minimums of the R-5 district and are close to the R-2 minimums that both the Greystone and South Hills subdivision adhere to so that the average pedestrian would not notice the difference. Staff has had general discussions with Neumann on this item and how to best accomplish this proposal. Staff has considered if Neumann should pursue a PUD or seek R-5 zoning with more strict requirements for setbacks within the Developer’s Agreement/Restrictive Covenants than what R-5 currently allows to ensure this project and Greystone appear harmonious. Staff has also had informal discussions with the City Attorney on this item and the general preference is to have base R-5 zoning for this development with the proposed setbacks within the Developer’s Agreement/Restrictive Covenants.

Multi-Family Residential Zoning:
As for the multi-family residential component, it is likely that R-4 would be the appropriate zoning district, as R-4 allows up to 12 dwelling units per structure. If the developer would like to seek more than 12 units per structure, they will have to seek R-6 zoning. Regardless, the current concept plan does not indicate setbacks, building height, underground parking, etc. Staff recommends the developer ensure they are meeting zoning district standards in regards to these items as they move forward and discuss the plan with staff.

Roadways/Access:
The concept plan is showing connection points off of STH 67 as well as a direct connection from Trillium Avenue in the Greystone subdivision. A roadway stub on the northeast corner of the development is also shown should future development occur on that property.

The main subdivision access is shown off STH 67 at the intersection with CTH E. The Wisconsin Department of Transportation (WisDOT) is the permitting authority for this access. Neumann has communicated they have a good understanding of where the State will issue access and feels as though this connection point will be permitted. The City will require permitting from the State for this access should the project move forward.

Residential Parking:
City code requires at least 2 parking spaces per each dwelling unit in the R-2, R-4, and R-5 zoning districts. R-6 requires 1.5 spaces per dwelling unit. Neumann will have no problem meeting this requirement for the single-family portion of this project as each of their home models feature at least a 2 car garage and private driveway suitable for parking. The future multi-family component will have to be designed to meet these minimums depending on the zoning district.

Park Area:
Neumann is showing a ~13-acre public park on the eastern end of the development. This includes the preservation of a small wetland and stormwater management on the site. City staff believes this area of the City is underserved in regards to parks and this development is a great opportunity to install a park that would service both this development and the Greystone subdivision to the north. Should the project move forward, the City will continue to have discussions on the park portion of the project.

Public Improvements/Department of Public Works:
The concept plan shows nearly one mile of roadways that will be built to City standard and dedicated to the City. The applicant will also be responsible for the installation of sidewalks within the development that meet City standard. Municipal sewer and water connections would be connected from the Greystone subdivision to the north.

Wetlands:
A small wetland is suspected in the northeast quadrant of the site per Sheboygan County GIS and is noted and avoided on the supplied concept plan as part of the designated park area. A wetland delineation for the site should be supplied by the developer as part of the approval process for the subdivision.

Plymouth Fire Department:
Fire Chief Pafford reviewed the initial plan and requested a greater turnaround radius for the stubbed roadway on the northeastern corner of the development for emergency vehicles, which has since been revised to show a hammerhead turnaround. Understanding the intention of the stub is to eventually connect to any future development to the north, staff is comfortable with this option. It will also need to be designed to accommodate busses, snow plows, etc.

TIF Application:
Neumann has submitted a TIF application to support this project should it move forward. That will be reviewed and discussed at the Common Council. There is presently no TIF district in place at this location within the City.

Potential Process/Future Applications:
Due to the projects scale and relative complexity, it will require at the minimum the following applications/approvals by the Plan Commission and Common Council. The order of operations for this project may change as the project evolves.

- **Comprehensive Plan Amendment:** Due to the property's future land use designation within the City's Comprehensive Plan, the applicant must seek a comprehensive plan map amendment application to change the land use designation from agricultural to residential.
    - Comprehensive Plan Amendments require Plan Commission recommendation and final approval by the Common Council.
- **Rezoning Petition:** Rezone property from agricultural to desired residential zoning district.
    - Applicant may also seek 2 lot certified survey map (CSM) in unison with the rezoning petition to divide the single family portion of the project from the multi-family portion and petition for appropriate zoning districts at that time for the final project.
    - Rezones (and certified survey maps) require both Plan Commission recommendation and final approval by the Common Council.
- **Preliminary Plat:** Applicant to provide preliminary plat for consideration by Plan Commission only
- **Final Plat:** Applicant provides final plat. Recommendation by Plan Commission and final approval by Common Council.

In addition to this process, the developer will also be required to enter into a stormwater management agreement with the City as well as a Developer's Agreement for the public improvements and any potential TIF incentives.

Staff Recommendation:
This is not a formal application. Staff recommends the Plan Commission discuss the project with the applicant, ask questions, and provide general feedback.

Copies Mailed/Emailed To:
I. Ryan Fritsch: rfritsch@neumanndevelopments.com

Attachments:
I. Applicant Materials

8/7/2025

City of Plymouth
C/O Jack Johnston
123 Smith Street
Plymouth, WI 53073

Dear Jack,

In conjunction with a proposed 119-lot single family residential subdivision and a 60-unit multifamily community, Neumann Developments, Inc. is submitting for discussion a conceptual site plan for approximately 67 acres off State HWY 67 in the City of Plymouth, tax key 59271829210.

Neumann Developments, Inc. was founded in 2000 and has developed over 6,000 for-ownership homesites, paved over 70 miles of roads, and preserved 3,000 acres of green space. Harbor Homes, a sister homebuilding partner of Neumann Developments, leads southeast Wisconsin in single family home construction volume and would serve as the exclusive homebuilder within the subdivision. Through strategic partnerships with the region's largest homebuilding operation and contractor network, Neumann Developments creates high quality developments that bring lasting value to communities.

Site Design

The property is located north of State HWY 67 and south of the Greystone Settlement subdivision; it is currently owned by the Plymouth Industrial Development Corporation and within the City's southernmost limits. The conceptual site plan contemplates an extension of CTH E north of State HWY 67, as well as a connection to the existing Trillium Lane, and a road stub at the northeast corner of the property that would serve a potential future through street. Multifamily units would be located along State HWY 67 on approximately 11.5 acres. 13.4 acres of park space is planned to the east of the multifamily space, while single family homes would be located to the north and west, providing a seamless transition from residential to the north. Approximately .55 acres of wetlands are present and intended to be preserved.

The proposed lot characteristics allow for flexibility that provide housing options meeting expectations of today's homebuyers. The sizing provides practical yard space and serves modern floor plans while ensuring attainable price points are met. Furthermore, the proposed characteristics lead to the preservation of site-specific open space more than what traditional zoning would allow. Open space is methodically planned by clustering lots in a fashion that retains existing environmental features and adds permanent green space, including multi-use parkland that would serve as a community amenity. Stormwater facilities would be accessible from right-of-way or access easements for maintenance purposes. All stormwater facilities and wetlands would be located within outlots for long-term management and protection.

The right-of-way profile would consist of the City's standard roadway specifications and engineering design standards. Right-of-way is identified on the plans and would ultimately be dedicated to the City for public use. The proposed road network provides sufficient access for public safety services and encourages efficient vehicular and pedestrian traffic flow serving both this subdivision and the surrounding area. The curvilinear road pattern slows vehicular traffic while remaining efficient and providing a natural, less monotonous aesthetic. Overall, the site plan strengthens both connectivity and walkability for the area.

Housing

Neumann Developments will usher the site from farmland to ready homesites while Harbor Homes would rapidly deliver homes to future residents by combining a speculative building process with a customer interface for to-be-built orders. The planned product type will consist of both ranch and two-story homes ranging from 1600sf – 2300sf with modern layouts and finishes. Homes will have 3-4 bedrooms, 2-3 bathrooms, and an attached 2-car, 2.5-car, or tandem 3-car garage. This housing style and price point can fit the needs of multiple demographic groups, including first-time home buyers, young families, and empty nesters. Protective covenants would be recorded and require architectural elements and private landscaping treatments in excess of municipal ordinances that sustain valuations and maintain an attractive neighborhood appeal. Full home-lot packages are anticipated to be around $400K.

Premier Real Estate (PRE/3) plans to develop a 60-unit multifamily complex consisting of five, two-story buildings within a privately owned and maintained space, appealing to both young families and empty nesters. PRE/3 has seen demonstrated demand for multi-family rentals in similarly sized communities and anticipates robust demand for this development. Units offered will offer all 2-bedroom / 2-bath apartments that are approximately 1150 SF in two different floor plans. All units would have attached garages, patios or balconies, upscale finishes, laundry in-unit, and adequate storage. All driveways and parking lots within the community would be privately managed.

Mixed-use residential provides a variety of housing products with varied price points that fulfill the needs and lifestyles of a wider percentage of residents.

Outcome

This petition is being made after careful consideration regarding the market demand in the Plymouth area. Along with the unique partnership with the City, we expect to bring in new members to the community and expand for-ownership options for years to come. This partnership is necessary to deliver a financially feasible housing product in today's market. The variety of attainable housing options aids in accommodating growth for Plymouth and provides options for new and relocating employees to meet the needs of local employers. Along with added property value, new families would support businesses and personal income through purchases of local goods and services, and they enrich the community with energy and ideas. Some buyers are expected to be current City residents as well, thus freeing up existing housing and increasing the overall market supply.

Thank you for your consideration. We look forward to partnering with the City of Plymouth on this generational opportunity.

Sincerely,

Ryan Fritsch

Ryan Fritsch
Neumann Developments, Inc.

CONCEPT PLAN

HWY 67 Development

Plymouth, Wisconsin

NEUMANN DEVELOPMENTS, INC.

SITE DATA TABLE

- Total Area	66.58 ac
- Wetland = 0.55 ac	
- Proposed Development	
- Single Family =	119 lots
- Multi-Family =	60 units
- Total =	179 units
- Density = 179 / 48.71 = 3.67 un/ac	
- Excludes park & storm water open space areas	
- Public Road = 5,526 lf (46.43 lf/lot)	

Proposed Single Family Zoning:
PUD - Planned Unit Development

- Minimum Lot Area = 8,700 sf
- Minimum Lot Width = 70'
- Minimum Building Setbacks:
  - Front Setback ..... 25'
  - Front Side Corner ..... 15'
  - Side Yard ..... 7.5'
  - Rear Yard ..... 25'

TRIO DESIGN • LAND SURVEYING • CIVIL ENGINEERING

NORTH

0 50 100 200

Scale: 1" = 100' (22"X34")

Scale: 1" = 200' (11"X17")

DATE: 08-21-2025

Architectural Elevations

Examples for Illustrative Purposes Only

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Plymouth, WI 53073-0107

Telephone: **(920) 893-3745**
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Web Site: **plymouthgov.com**

DATE: August 26, 2025

TO: Plan Commission

FROM: Jack Johnston, Assistant City Administrator/Community Development Director

RE: **Fee Schedule Update:** Staff seeking recommendation to update the Zoning Fee Schedule last updated in 2020

Background:
The Zoning Fee Schedule was last updated in 2020 and needs to be updated after the City passed a new zoning ordinance regarding additions to existing non-conforming structures and substandard lots. The proposed fee would be used to cover the cost of publication of notices in the newspaper and to neighbors within the vicinity of any future applications.

It also includes updated language that concept fee application does not carry a charge as well puts a cost of $250 for "Plan Commission Determination of Use" as stipulated in Sec. 13-1-111 in the Plymouth Municipal Code. Further, some language in the fee schedule is proposed to be updated to reflect existing ordinance language that allows the City to bill back the cost of professional consultant time related to applications in order to avoid confusion and make that clear going forward. Finally, staff has proposed removing the fee for "temporary signs" which is currently in the fee schedule. "Temporary signs" and banners are regulated differently in the code. Temporary signs refer to signs typically posted in one's yard like a campaign sign or "For Sale" sign.

Staff Recommendation:
Staff recommends the Plan Commission recommend approval of the fee schedule as presented. Staff would then present this to the Common Council at a future meeting as a resolution.

Attachments:
I. Proposed Fee Schedule

DESCRIPTION	APPROVED FEE
Amendments to the Text of Zoning Ordinance	$325.00
Amendments to the Official Zoning Maps (Rezoning Petitions)	$325.00
Comprehensive Plan Revision to the Text	$450.00
Comprehensive Plan Revision to the Maps	$450.00
Zoning Permits for Conditional Uses - Principal Structure	$250.00
Zoning Permits for Conditional Uses - Accessory Uses/Structure	$250.00
~~Temporary Sign Permit~~	~~$40.00~~
Sign Permits (for signs which are Permitted Uses)	$70.00
Banner Signs	$25.00
Fence	$35.00
Site Plan Review Fees:	
a. New Commercial Start	$325.00
b. Commercial Additions Under 25,000 sq. ft.	$225.00
c. Commercial Additions Over 25,000 sq. ft.	$325.00
d. Commercial Accessory Uses	$125.00
e. Commercial Site Plan Alterations	$125.00
f. New Residential (1 or 2 Family)	$25.00
g. New Residential Over 1 Building - Per Unit	$50.00 (minimum cost $600)
h. Mobile Homes Park (per each 50 sites or part thereof)	$500.00
~~Zoning Certificates of Temporary Occupancy (Requested)~~	~~$25.00~~
~~Zoning Certificates of Occupancy~~	~~No Charge~~
Application for Variance	$500.00
Request for Zoning Interpretation (In- Writing)	$25.00
Appeals of Zoning Decisions	$500.00
Application fee for Planned Development Districts ~~(City may impose additional fees to cover the cost of review by a professional engineer, legal advisor, or other professional municipal consultant)~~	$500.00
Driveway Permit (Second Driveway Only)	$65.00
Wireless Telecommunications Permit	$150.00

Wind Energy Conversion System Permit	$150.00
Bed and Breakfast Permit	$65.00
Detached Heating System (annual)	$60.00
In-Ground Swimming Pool	$150.00
Above-Ground Swimming Pool	$75.00
Certified Survey Map (CSM)	$65.00
Preliminary Plat Approval	$250.00
Final Plat Approval	$250.00
Special Meetings (at request of private party)	$500.00
Addition to Non-Conforming Structure/Construction on Existing Substandard Lot	$65.00
Concept Plan	No Charge
Classification of Unlisted Uses; Determination by Plan Commission	$250

Note: Sec. 13-1-174 of City of Plymouth Code of ordinance allows the city to impose additional fees above and beyond the base fee to cover the cost of review by a professional engineer, legal advisor, or other professional municipal consultant.