

# City of Plymouth Plan Commission

**Members: Please notify us if you are unable to attend the meeting.**

Chairman; Mayor Don Pohlman

Members:

Ron Nicolaus (Vice Chairman)

Angie Matzdorf

Jane Meyer

Jeremy Schellin

John Wyatt

Justin Schmitz

The City of Plymouth Plan Commission will have a meeting on **Thursday February 5, 2026** at 6:00 PM in **Room 305**, City Hall, 128 Smith St, Plymouth, Wisconsin.

The agenda will be as follows:

- 1.) Call to Order & Roll Call
- 2.) Approval of Minutes from January 15, 2026
- 3.) **Recommendation of Extraterritorial Certified Survey Map;** W5996 Sumac Road (Parcel numbers 59016222311 and 59016222351 – located on Sumac Road in the Town of Plymouth. Prange (enclosure)
- 4.) **Public Hearing and Possible Action: Existing Non-Conforming Structure Addition Seeking Approval;** 207 S. River Blvd; Errol Jones, seeking approval for a lateral addition to an existing non-conforming structure as allowed per PMC 13-1-82(b)(2). Jones (enclosure)
- 5.) **Preliminary Plat Seeking Approval;** Consideration of a preliminary plat of subdivision on along STH 67 on the City's southwest side (parcel 59271829210) to be known as Heritage Grove. Neumann Developments, Inc. (enclosure)
- 6.) **Communication – Letters, E-mails, or reports Related to the Plan Commission** (Chairman, Secretary, Plan Commission Members, City of Plymouth Staff/Alderspersons)
- 7.) Adjournment

It is possible that members of and possibly a quorum of members of other governmental bodies of the municipality may be in attendance at the above stated meeting to gather information. No action will be taken by any governmental body at the above stated meeting other than the governmental body specifically referred to above in this notice. Please note that, upon reasonable notice, efforts will be made to accommodate the needs of disabled individuals through appropriate aids and services. For additional information or to request this service, contact City of Plymouth ADA Coordinator Leah Federwisch, located in the Plymouth Utilities office at 900 County Road PP, Plymouth, WI or call 920-893-3853

# City of Plymouth Plan Commission

## UNOFFICIAL MINUTES

Mayor Pohlman called the meeting to order at 6:00 PM on January 15, 2026. The following members were present: Jane Meyer, Jeremy Schellin, Jeremy Schmitz, Ron Nicolaus, John Wyatt, and Angie Matzdorf. Also present: City Administrator/Utilities Manager Tim Blakeslee, Assistant City Administrator/Community Development Director Jack Johnston, Ehlers Consultants Kayla Thorpe and Paul Boening.

- 1.) **Call to Order & Roll Call:** Mayor Pohlman called the meeting to order at 6:00 pm. The meeting began with a roll call of the Plan Commission.
- 2.) **Approval of Minutes from December 4, 2025.** Motion was made by Nicolaus/Schmitz to approve the minutes. Upon the call of the roll, all voted aye. Motion carried.

3.) **Discussion and Recommendation of Extraterritorial Certified Survey Map; N5908 Willow Road and parcel 59016218990 located in the Town of Plymouth. Van Horn Automotive (enclosure)**

Van Horn Automotive has submitted an application for approval of a certified survey map in the Town of Plymouth that is located in the City of Plymouth Extraterritorial Review jurisdiction (1.5 miles from City of Plymouth boundaries). The land division has already been approved by the Town of Plymouth and seeks create a new parcel that would allow Van Horn to expand their business operation west.

Jeff Niesen of Van Horn Automotive Group was present and explained that CSM was to partition off 5 acres of vacant land owned by the Kleinhans Trust to future site expansion for their business operation. No concrete plans for that land are being contemplated at this time however.

Mayor Pohlmann asked if this had received Town approval. Assistant City Administrator Johnston explained the CSM had received Town approval at a meeting earlier in December.

Motion was made by Schmitz/Nicolaus to recommend the Common Council approve the extraterritorial certified survey map for N5908 Willow Road and parcel 59016218990 located in the Town of Plymouth as presented. Upon the call of the roll, all voted aye. Motion carried.

4.) **Public Hearing regarding the proposed creation of Tax Incremental District No. 9, the proposed boundaries of the District, and the proposed Project Plan for the District.**

Mayor Pohlman declared a public hearing a 6:05 pm. City Administrator Blakeslee briefly introduced the item, explaining the proposed TIF district is a single parcel industrial TID that encapsulates the newly proposed Sargento manufacturing facility that received Plan Commission approval last summer. The TIF and planned Sargento facility would help facilitate and fund needed public improvements such as a new well, water main loop, sanitary force main, etc. Sargento's would receive 85% of newly generate tax increment up to a mutually agreed upon cap. Kayla Thorpe from Ehlers detailed more on the project plan for TID, also adding that the "but for" test has been applied, indicating that this

development as presented, including that this development as presented, including the planned public improvements, would not occur without the support of TIF.

With no further comments from the public, Mayor Pohlmann closed the public hearing.

**5.) Consideration and possible action on a “Resolution Establishing the Boundaries of and Approving the Project Plan for Tax Incremental District No. 9.”**

This continues from item #4. Commissioner Nicolaus had a question on the well and the water facilities being sized to accommodate Sargento’s need. City Administrator Blakeslee explained Kapur was working on those designs in the interim, but this was considered and Sargento will be funding part of these upgrades with the new facility.

Justin Schmitz asked about planned roadway improvements. City Administrator Blakeslee that none were contemplated as part of the TID project plan due to the property lying on a state highway and county highway. However, it is expected that Sargento will install a turn lane onto their property along CTH PP.

Jeremy Schellin asked about the 85% increment. City Administrator Blakeslee explained Sargento would receive 85% of the newly created tax increment for building the site after the City’s debt service was paid back for noted public improvement projects, up to a cap.

Motion made by Nicolaus/Wyatt to approve “Resolution Establishing the Boundaries of and Approving the Project Plan for Tax Incremental District No. 9.” Upon call of the roll, all voted aye with Commissioner Schellin abstaining. Motion carried.

**6.) Public Hearing regarding the proposed creation of Tax Incremental District No. 10, the proposed boundaries of the District, and the proposed Project Plan for the District.**

Mayor Pohlman declared a public hearing a 6:21 pm. City Administrator Blakeslee briefly introduced the item, explaining the proposed TIF district is a mixed use TID district with proposed plans for a 119-lot residential subdivision by Neumann Developments, Inc and a 60-unit multi-family residential project by PRE/3. Due to state law requiring newly platted residential within a TID to be capped at 35% of the gross acreage, additional land was annexed south of STH 67 to Sumac Road as well as the inclusion of the Greystone condominiums.

The proposed projects would help bridge the gap based on the identified housing shortage from the 2024 Housing Study. The generated increment would help fund public projects identified in the project plan. Kayla Thorpe from Ehlers detailed more on the project plan for TID, also adding that the “but for” test has been applied, indicating that this development as presented, including that this development as presented, including the planned public improvements, would not occur without the support of TIF.

With no further comments from the public, Mayor Pohlmann closed the public hearing.

**7.) Consideration and possible action on a “Resolution Establishing the Boundaries of and Approving the Project Plan for Tax Incremental District No. 10.”**

This item continues from #6. Commissioner Nicolaus asked about utility infrastructure needed for this development such as the new water main. City Administrator Blakeslee says that is being designed and will be installed by the developer as part of the developer's agreement.

Commissioner Schellin asked about general protections and exposure for the City for this project. City Administrator Blakeslee responded this is a pay-go TID in which the developer will be installing the proposed infrastructure and that the risk to the City is minimal as the developer has agreed to take on the early cost of the infrastructure and any new increment generated would only be after those improvements are installed. Kayla Thorpe from Ehlers also noted that since the City projects are funded over 20 years, the exposure to the taxpayer is very miniscule.

Motion by Schellin/Wyatt to approve Resolution Establishing the Boundaries of and Approving the Project Plan for Tax Incremental District No. 10." Upon call of the roll, all voted aye. Motion carried.

**8.) Communication – Letters, E-mails, or reports Related to the Plan Commission (Chairman, Secretary, Plan Commission Members, City of Plymouth Staff/Alderspersons)**

None.

**9.) Adjournment**

Motion made by Nicolaus/Schmitz to adjourn the meeting at 6:29 pm. Upon the call of the roll, all voted aye. Motion carried.





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**DATE:** January 21, 2026

**TO:** Plan Commission

**FROM:** Jack Johnston, Assistant City Administrator/Community Development Director

**RE:** **Recommendation of Extraterritorial Certified Survey Map;** W5996 Sumac Road (Parcel numbers 59016222311 and 59016222351 – located on Sumac Road in the Town of Plymouth. Prange (enclosure)

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**Background:**

Randall and Denise Prange have submitted a certified survey map (CSM) for extraterritorial review and approval by the City of Plymouth due to the property being within the 1.5 mile extraterritorial review area of the City of Plymouth. The property itself is located within the Town of Plymouth and received Town approval on January 13, 2026.

**Current Configuration vs. Changes:**

The Prange's currently have their home at W5996 Sumac Rd in the Town of Plymouth. The property's configuration is on two different tax parcels, a portion of the home actually encroaching on the lot line of the two parcels.

The CSM corrects this issue and creates a succinct parcel that does not have the encroachment issues. The new parcel is approximately 5.94 acres in size (including Sumac Road R-O-W).

**Sheboygan County Approval:**

The Town of Plymouth approved this CSM on January 13, but it also requires Sheboygan County review. The applicant is working through approval of the CSM through Sheboygan County as well at this time.

**City Approval Process:**

The City's Common Council would ultimately be the body to give approval of the CSM from the City of Plymouth. However, the Plan Commission must first furnish a recommendation to the Council.

**Staff Recommendation:**

The property is not located near adjacent City lands or roadways. As such, staff recommends the Plan Commission recommend approval the extraterritorial CSM for the property as presented. Final approval of the CSM is scheduled to be held at the Common Council meeting at a later date.

**Copies Mailed/Emailed To:**

- I. Denise Prange: [deniseprange@yahoo.com](mailto:deniseprange@yahoo.com)

**Attachments:**

- I. Certified Survey Map

5901622311

W6018

W5996

W5961

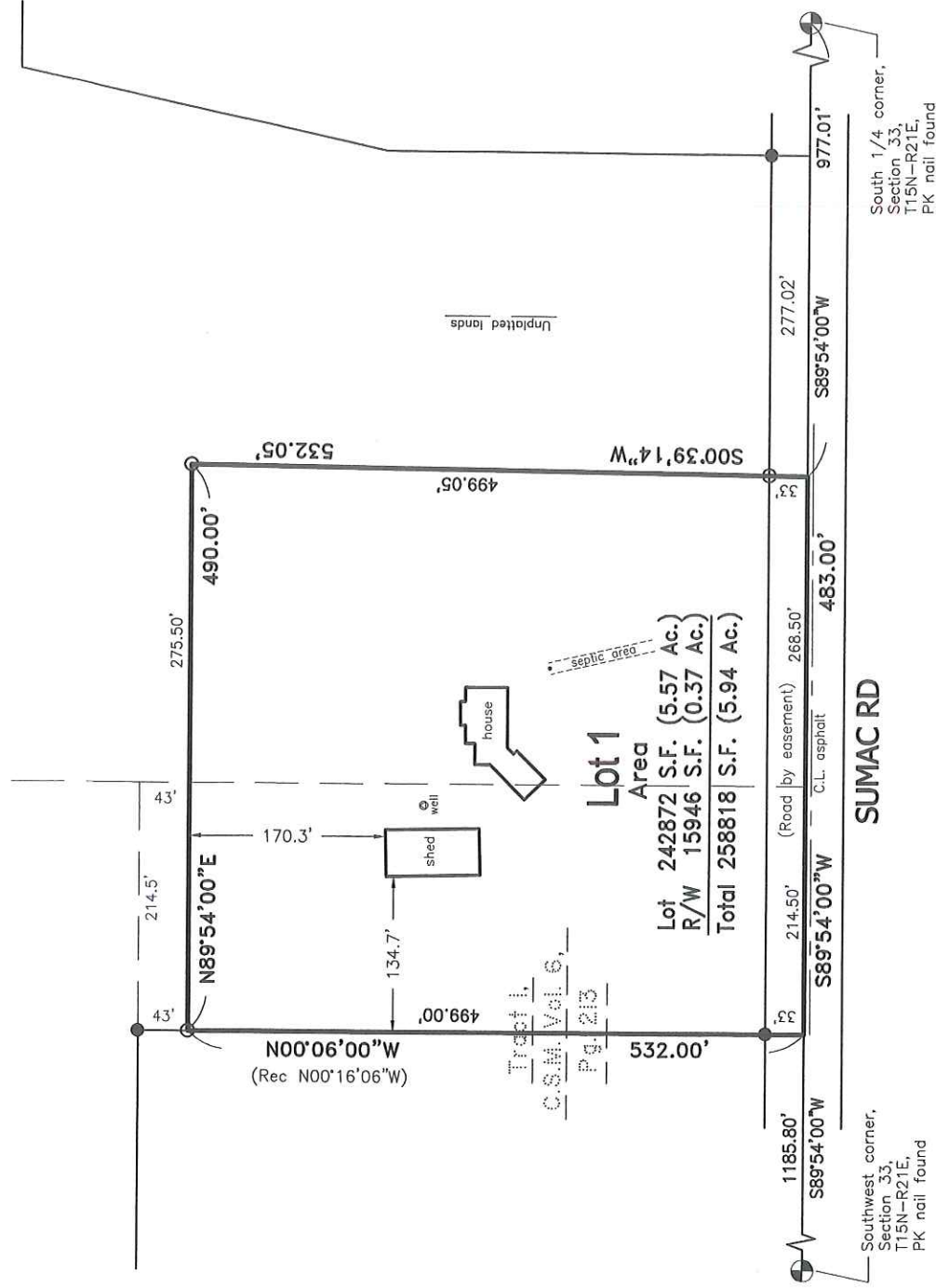
Sumac Rd

Page 7 of 33

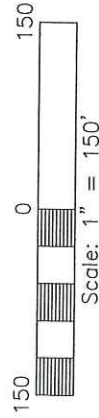
# CERTIFIED SURVEY MAP

Part of Tract 1 of a Certified Survey Map recorded in Volume 6,  
Page 213 of C.S.M.'s and part of the Southeast 1/4 of the  
Southwest 1/4 of Section 33, T15N-R21E, Town of Plymouth,  
Sheboygan County, Wisconsin.

Page 1 of 2



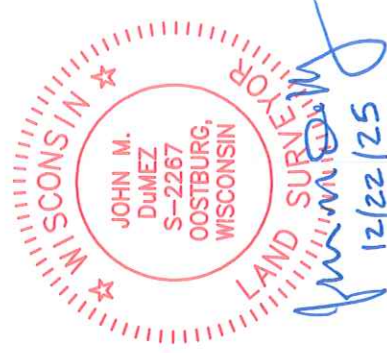
(Sheb. Co. Coordinate System)  
Bearings referenced to the  
south line of the SW 1/4 of  
Sec. 33 bearing  $S89^{\circ}54'00''W$



## Legend

- = 1"x18" iron pipe weighing min. 1.13 lbs/ft, SET
- = 1" iron pipe, FOUND
- ⊙ = County Section corner, FOUND

Name of owner: Randall & Denise Prange  
Date of survey: Dec. 22, 2025



Prepared by:  
**campsite**  
Surveying & Mapping  
Oostburg, WI 53070  
(920) 564-6812



**CERTIFIED SURVEY MAP**

STATE OF WISCONSIN)

§

SHEBOYGAN COUNTY )

**PAGE 2 OF 2**

**SURVEYOR'S CERTIFICATE OF COMPLIANCE**

I, John M. DuMez, Wisconsin Professional Land Surveyor, hereby certify that under the direction of Randall & Denise Prange, I have surveyed, divided, and mapped part of Tract 1 of a Certified Survey Map recorded in Volume 6, Page 213 of C.S.M.s and part of the Southeast ¼ of the Southwest ¼ of Section 33, T15N-R21E, Town of Plymouth, Sheboygan County, Wisconsin. The parcel is described as follows:

Commencing at the South Quarter corner of Section 33, T15N-R21E; thence along the south line of the Southwest ¼ of said Section 33, S89°54'00"W 977.01 feet to the point of beginning; thence continuing along said south line, S89°54'00"W 483.00 feet; thence N00°06'00"W (recorded as N00°16'06"W) 532.00 feet; thence N89°54'00"E 490.00 feet; thence S00°39'14"W 532.05 feet to the point of beginning.

SAID PARCEL CONTAINS 258818 SQ. FT. (5.94 AC.) OF LAND AND IS SUBJECT TO UNRECORDED AND RECORDED EASEMENTS AND RESTRICTIONS.

I hereby certify that the map is a correct representation of all exterior boundaries of the land surveyed. I do further certify that I have fully complied with Section 236 of the Wisconsin Statutes, the Subdivision Ordinance of Sheboygan County, and the Town of Plymouth Zoning Ordinances in surveying and mapping the same.

Dated this 22<sup>nd</sup> day of December, 2025.

  
John M. DuMez

Wisconsin P.L.S S-2267



**OWNER'S CERTIFICATE**

As owner(s), I (we) hereby certify that I (we) caused the land described herein to be surveyed, divided, and mapped as represented on this Certified Survey Map. I (we) also certify that this Certified Survey Map is required to be submitted to the following for approval:

**Town of Plymouth**

**City of Plymouth (extraterritorial jurisdiction)**

**Sheboygan County Planning Department**

  
Randall H. Prange (OWNER)

  
Denise F. Prange (OWNER)

Dated this 6 day of January, 2026.

**CERTIFICATE OF THE TOWN OF PLYMOUTH**

This Certified Survey Map has been submitted to and approved by the Town of Plymouth.

  
Warren Suedke

- Town Chairperson

  
Debra Schund

- Town Clerk

Dated this 13 day of January, 2026.

**CERTIFICATE OF THE CITY OF PLYMOUTH (extraterritorial jurisdiction)**

This Certified Survey Map has been submitted to and reviewed by the City of Plymouth.

- Mayor

- City Clerk

Dated this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

**CERTIFICATE OF SHEBOYGAN COUNTY PLANNING DEPARTMENT**

This Certified Survey Map has been reviewed and approved by the Sheboygan County Planning Department.

Title: \_\_\_\_\_ Dated this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.



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**DATE:** January 28, 2026

**TO:** Plan Commission

**FROM:** Jack Johnston, Assistant City Administrator/Community Development Director

**RE:** **Existing Non-Conforming Structure Addition;** 207 S. River Blvd; Errol Jones, seeking approval for a lateral addition to an existing non-conforming structure as allowed per PMC 13-1-82(b)(2)

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**Background:**

Last year, the Plan Commission approved an ordinance change to allow certain lateral and vertical additions to existing non-conforming structures with the City. Previously, the municipal code was very restrictive in this area. The Plan Commission spent several meetings fine tuning the ordinance change to give property owners some relief in this area.

The application before the Plan Commission at this time is for an addition to a detached garage at a multi-family residential property located at 207 S. River Blvd. The garage is considered non-conforming due to property line setbacks. In this case, the garage is only 10' at its closest point to the neighboring lot line and code requires it to be 25' for rear yards.

**Proposed Addition Details and Non-Conformity:**

The addition seeks to add approximately 340 square feet to an existing detached garage this is approximately 981 square feet in size currently. The garage is considered existing nonconforming due to PMC 13-1-140(b)(2)(a)(3) which requires any detached garage over 600 square feet to comply with the setback restrictions of the principal structure. In the R-4 multi-family residential district, setbacks for principal structures are 15' for side yards and 25' for rear yards. As such, if the garage was conforming, it would have a 25' setback on both the southern and eastern property lines. The proposed addition is wholly within the side yard which would require a 15' setback if it was a standalone structure.

**Non-Conforming Addition Language:**

The ordinance language that allows lateral additions to existing non-conforming structures reads as the following:

- Lateral additions are permitted if the plan commission finds the addition meets existing zoning requirements as established in the respective zoning district at the time of application as though it was a new structure, regardless of the established existing-non-conforming structure to which it will be attached.

The proposed addition is wholly within the property's *side* yard, not the *rear* yard, which would require a 15' setback if this was a new structure. The proposed site plan does show a 15' setback from the neighboring lot line which is in conformity with the R-4 district and the addition language as shown above.

As the property is a corner lot that has dual access off both Ann Ct and South River Blvd, a site map of the property showing which areas are front yards, side yards, and rear yards are included to help the Plan Commission understand why the addition requires only a 15' setback in this case because it is in a side yard.

**Standards for Approval:**

The ordinance requires that the Plan Commission may only approve an addition to an existing non-conforming structure if it finds the following are true/present:

- The proposed addition is appropriate in relation to the physical character of the site and the usage of adjoining land areas.
- The layout of the proposed addition with regard to entrances and exits to public streets; the arrangement and improvement of interior roadways; the location, adequacy and improvement of areas for parking and for loading and unloading; and the traffic pattern generated by the proposed construction are developed in a manner consistent with the safety of residents and the community, and the construction or use minimizes any traffic hazard created thereby.
- The proposed water supply, drainage facilities and sanitation and waste disposal will be adequate.
- The landscape and appearance of the proposed addition is consistent with the surrounding properties. The plan commission may require that those portions of all front, rear and side yards not used for off-street parking to be attractively planted with trees, shrubs, plants or grass lawns, and that the site be effectively screened so as not to impair the value of adjacent properties nor impair the intent or purposes of this section.

**Staff Recommendation:**

Staff requests the Plan Commission hold a public hearing for this item and then approve the application as presented. The motion to approve should include a note that the standards for approval have been satisfied.

**Copies Mailed/Emailed To:**

- I. Errol Jones: [errol.jones79@yahoo.com](mailto:errol.jones79@yahoo.com)

**Attachments:**

1. Applicant Materials
2. Yard Map

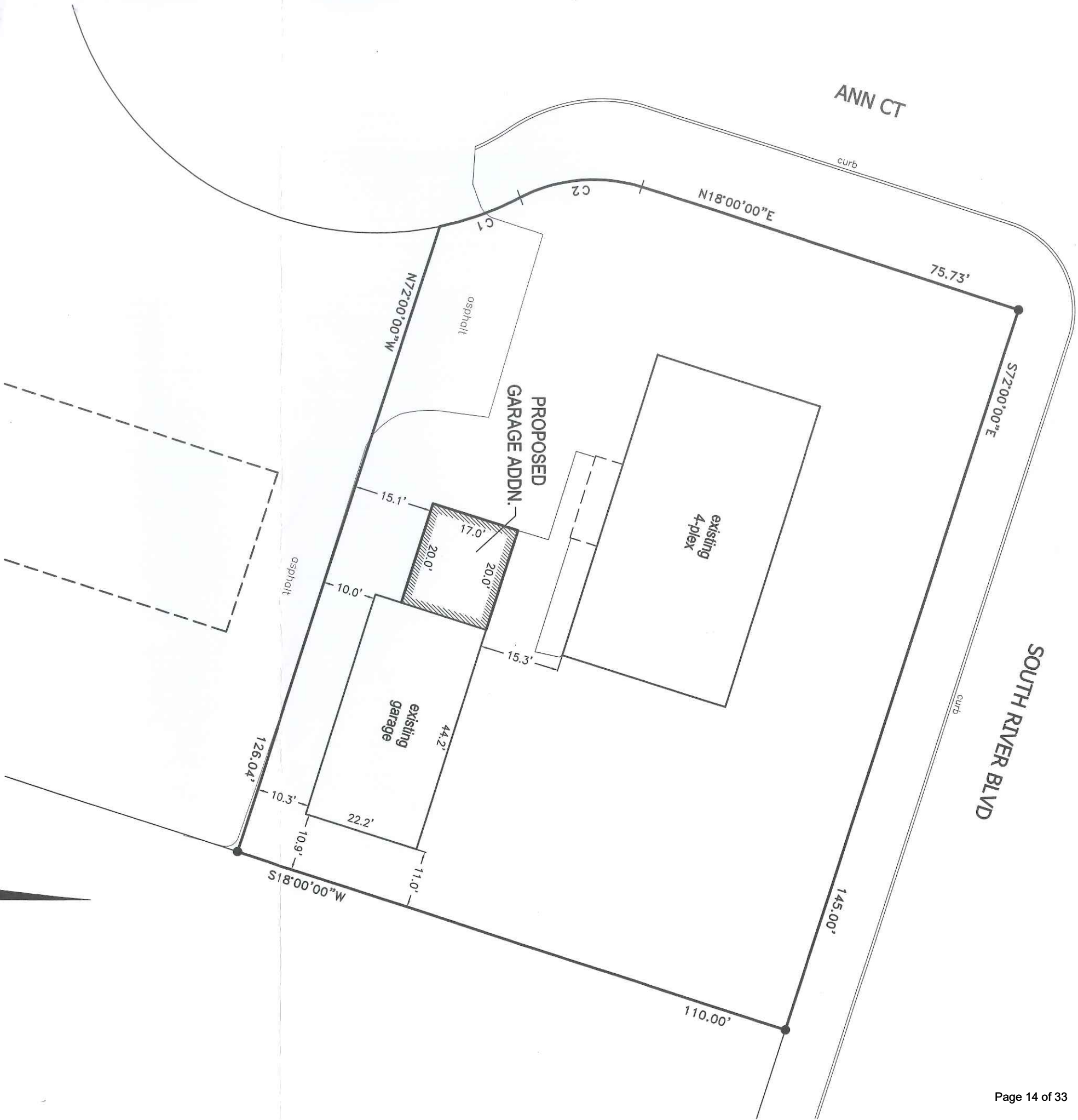




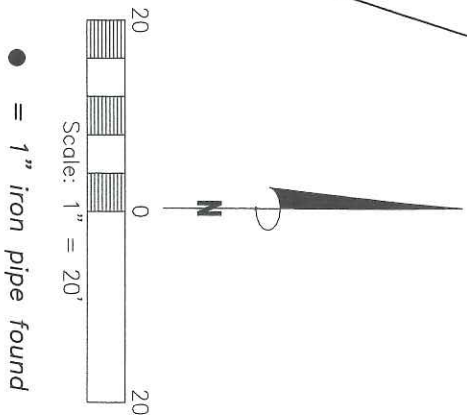








CURVE NUMBER	C1	C2
LENGTH	16.31	23.86
RADIUS	60.00	30.00
DELTA	15°34'18"	45°34'18"
CH BEAR	N19°47'09"W	N04°47'09"W
CHORD LENGTH	16.26	23.24



Plat of Survey  
for  
Errol Jones

Lot 38, Plat of South River Village, City of Plymouth, Sheboygan County, Wisconsin.

I, John M. DuMez, Wisconsin Professional Land Surveyor, certify that I have surveyed the described property and that the map shown is a true and accurate representation thereof to the best of my knowledge and belief.

*John M. DuMez*

Sept 10, 2025

Date

John M. DuMez - Wisconsin P.L.S. S-2267

The certification contained on this document shall not apply to copies.



**Compsite**  
Surveying & Mapping  
Oostburg, Wisconsin  
(920) 564-6812

TAX PARCEL NO. 59271822842  
ADDRESS: 207 S. River Blvd  
PATH: c:\projects2\jones\_erol\207SRiverBlvd\dwg\  
DRAWN BY: jdm  
PROJECT: 4785

[illegible]

Lot 38, Plat of South River Village, City of Plymouth, Sheboygan County, Wisconsin.

John M. DuMez - Wisconsin P.L.S. 5-2267  
The certification contained on this document shall not apply to copies.



TAX PARCEL NO. 50271822842  
ADDRESS: 207 S. River Blvd  
PATH: c:\projects2\jones\_ero\207SRiverBlvd\dwg\  
DRAWN BY: jdm  
PROJECT: 4785



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**DATE:** January 29, 2026

**TO:** Plan Commission

**FROM:** Jack Johnston, Assistant City Administrator/Community Development Director  
Tim Blakeslee, City Administrator/Utilities Manager

**RE:** **Preliminary Plat Seeking Approval;** Consideration of a preliminary plat of subdivision on along STH 67 on the City's southwest side (parcel 59271829210) to be known as Heritage Grove. Neumann Developments, Inc. (enclosure)

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**Background:**

Neumann Developments, Inc. has submitted the formal preliminary plat for the Heritage Grove residential subdivision on the City's southwest side along STH 67. The subdivision is within existing City limits and will be directly south of the existing Greystone subdivision. Low density residential and Town of Plymouth parcels are located to the east of the subdivision. Across STH 67 is agricultural land that was recently annexed from the Town into the City. The developers plan to build out the subdivision in four phases (phasing plan is attached).

The proposed preliminary plat is very similar to the concept plan that the Plan Commission has already seen. The developer is still proposing 119 single-family residential lots, but the road geometry has slightly changed due to the land's topography. In short, the cul-de-sac has moved more southwest from the original concept plan. The development includes 3.67 acres of land that will be dedicated to the City for parkland, along with five stormwater management ponds and eight total out-lots. The proposed multi-family is shown on the preliminary plat, but is not being undertaken by Neumann and not up for approval at this time.

**Preliminary Plat vs Final Plat**

Land divisions that create 5 or more new lots are considered major land divisions and are subject the preliminary and final plat process as dictated by Title 14, Chapter 1, Article D of the Plymouth Municipal Code. Land divisions of less than 5 lots can be accomplished though a certified survey map (CSM) and are held to different, less rigorous standards. As this plan calls for the eventual division into 94 residential lots, along with R-O-W dedication and out-lots, the preliminary plat process is the appropriate first step in this subdivision gaining approval.

The preliminary plat is the first step in the approval of a major land division. PMC 14-1-32(b) notes that the preliminary plat approval process is to be considered "an expression of approval or conditional approval of the layout submitted as a guide to the preparation of the final plat, which will be subject to further consideration by the plan commission and common council at the time of its submission." As such, final engineering documents, roadways designs, etc. are not required for the preliminary plat approval. If the preliminary plat is approved, the developer would then prepare and submit those necessary documents for consideration with the final plat at a future time. It is expected that each of the four phases of the subdivision will have a separate final plat.

**Comprehensive Plan/Zoning/Setbacks:**

Neumann applied for and received comprehensive plan and zoning approvals for the property in late 2025. The single-family portion of the project has R-5 Traditional Neighborhood zoning and their proposed lot minimums and setbacks adhere to the minimums in that district. Language included in the developers agreement with the City also spells out the setback restrictions shown on the preliminary plat and those minimums will be in the HOA documents for the subdivision.

**Roadways/Right-of-Way Dedication:**

The plan calls for the dedication of 5,526 linear feet of roadway for three new City roadways (Pioneer Place, Founders Way, and Colby Court). City code requires 60' R-O-W for local streets which are shown on the preliminary plat for all future roadways in the subdivision. Final roadway designs will be required to be submitted prior to final plat consideration for each of the phases. Those roadway designs and construction will be for the developer to complete. Both initial roadway designs and final construction will be reviewed by the City prior to considering accepting them as public improvements and must be design and built to City standard. The developer should work with the Department of Public Works to understand those requirements. The proposal also includes the installation of sidewalks on both sides of the new roadways.

**Traffic:**

The main subdivision entrance will be off of STH 67 and the CTH E intersection. As the City does not control access along STH 67, the developer is working with WisDOT to obtain necessary permits and they will determine if a turn lane will be required.

At the request of the Plan Commission, Neumann contracted with TADI to complete a traffic impact analysis (TIA) for the to see if the increased traffic would require any alterations to their design and safety measures along STH 67. TADI's findings were that intersection improvements were warranted. However, if more developments occur in this area, then changes to pavement marking and signing are recommended. As STH 67 is not controlled by the City, it will ultimately be up to WisDOT on updating markings and signing to these areas.

More detail of the review of the TIA is included as an attachment to this report.

**Stormwater/Erosion Control:**

Final stormwater/erosion control plans will have to be submitted with the final plat for each phase of the subdivision. Those documents will be reviewed and recommended for approval by City staff before consideration by the Plan Commission. The current preliminary plat shows five stormwater management ponds throughout the development that will be the responsibility of the future homeowner's association.

**Developers Agreement/TID #10**

At the January 27, 2026 Common Council meeting, the Common Council approved a developer's agreement with Neumann Development and is in the process of creating TID #10 to support the project. TID #10 is a Mixed-Use District encompassing approximately 193 acres in the southwest corner of Plymouth, located both north and south of Highway 67, and is intended to support a proposed 119-lot single-family subdivision by Neumann Development and a 60-unit multifamily development by Pre3.

Under the agreement, the 119 single-family homes will be built over four phases, with an 85% pay-as-you-go reimbursement of TID increment through a four-phase Municipal Revenue Obligation (MRO). Most of the MRO is tied to public infrastructure improvements (streets, water, sewer, sidewalks, electric, and a neighborhood park), and the agreement includes a lookback provision to align reimbursements with actual costs if they come in lower than estimates. Current projections estimate completion by 2033, while the remaining 15% of increment can be directed to other City priorities, including the sanitary force main upgrade, the WWTP Facility Plan, and the Highway 67 water main extension, with any additional increment potentially supporting a future municipal well and WWTP upgrades; the total project value is estimated at \$47.6 million,

and the agreement includes additional safeguards such as lot size restrictions, warranties, a letter of credit, inspection requirements, and other protections.

**Public Works/Plymouth Fire Department Review:**

DPW Director Austin has reviewed the plat and found the R-O-W dedications widths to be code compliant. Final engineering reports and associated documents will be required to be submitted and reviewed by the City at the time of final plat consideration for each phase and approved by the City.

During the concept plan phase, former Fire Chief Pafford reviewed the initial plan and requested a greater turnaround radius for the stubbed roadway on the northeastern corner of the development for emergency vehicles, which has since been revised to show a hammerhead turnaround. Understanding the intention of the stub is to eventually connect to any future development to the north, staff is comfortable with this option. It will also need to be designed to accommodate busses, snow plows, etc., which will be part of the review of the final plat when that phase is being contemplated.

**Engineer Review:**

Kapur is serving as the consulting engineer for City review on this project. Erik Gustafson of Kapur conducted a review of the submitted plat and supplied a review letter with nine (9) review comments to be addressed. The comments are minor in nature and City staff is comfortable with the Plan Commission to approve the preliminary plat contingent on these review comments being addressed to Kapur's satisfaction. The review letter is included as an attachment.

**Final Plat/Specific Implementation Plan/Next Steps**

Contingent on preliminary plat approval, the developer plans to bring forward a formal final plat and specific implementation plan (SIP) for each phase of the subdivision before they begin construction on that specific phase.

In addition to the final plat documents, the developer will also be required to submit the following materials as required by code and will be reviewed and approved by the Department of Public Works, City Engineer, and City Attorney. These documents can be reviewed and approved administratively by the appropriate City staff person.

- Erosion control plans.
- Final street, plans.
- Final storm sewer plans.
- Final sanitary sewer plans.
- Final water plans.
- Final grading plans.
- Final park plans.
- Drainage flows.
- Groundwater presence.
- Public land dedications.
- All easements.
- Any changes from the preliminary plat

City staff may also request other relevant plans/documents for review along with the final plat materials. Final plats and SIPs require both Plan Commission and Common Council approval.

**Staff Recommendation:**

Staff recommends the Plan Commission consider approval of the preliminary plat contingent on Kapur's engineering comments being addressed.

If approved, staff will work with the developer on necessary documents to be submitted along with any final plat alteration for each of the four phases. Final plat approval will first be reviewed and recommended for approval by the Plan Commission will eventual final action by the Plymouth Common Council.

**Copies Mailed/Emailed To:**

- I. Brian Lindgren: [blindgren@neumanncompanies.com](mailto:blindgren@neumanncompanies.com)
- II. Ryan Fritsch: [rfritsch@neumanncompanies.com](mailto:rfritsch@neumanncompanies.com)

**Attachments:**

- I. Narrative & Preliminary Plat
- II. Phasing Plan & supplemental applicant materials
- III. Kapur Preliminary Plat Review Letter
- IV. Kapur TIA Review Letter

Action	Date	Status
Plan Commission Meeting; Neumann Concept Plan	9/4/2025	Positive Feedback
Plan Commission Meeting; Rezone Request Neumann, Comprehensive Plan Amendment, CSM	11/6/2025	Rec. Approval
Public Notice: newspaper notice for Comprehensive Plan Amendment	11/7/2025	Published
Plan Commission Meeting; Rezone Request PRE/3	12/4/2025	Rec. Approval
Common Council Meeting; Comp Plan Amendment/ 2 Lot CSM	12/9/2025	Approved
Public Notice: Pre/3 & Neumann newspaper notice; mailer to neighbors/Town of Plymouth	1/9, 1/16 newspaper 1/14/2026 mailer	Published Mailed
Common Council Public Hearing & Action; PRE/3 & Neumann Rezone Public Hearing	1/27/2026	Approved
Neumann Preliminary Plat	2/5/2025	This meeting



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# HERITAGE GROVE

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1/5/2026

City of Plymouth  
C/O Jack Johnston  
128 Smith Street  
Plymouth, WI 53073

Dear Jack,

In conjunction with a proposed 119-lot single family residential subdivision known as Heritage Grove, Neumann Developments, Inc. is submitting for consideration a rezoning petition for approximately 56 acres located south of Greystone Drive, North of STH 67 in the City of Plymouth, tax key #59271829210.

Neumann Developments, Inc. was founded in 2000 and has developed over 6,000 for-ownership homesites, paved over 70 miles of roads, and preserved 3,000 acres of green space. Harbor Homes, a sister homebuilding partner of Neumann Developments, leads southeast Wisconsin in single family home construction volume and will serve as the exclusive homebuilder within the subdivision. Through strategic partnerships with the region's largest homebuilding operation and contractor network, Neumann Developments creates high quality developments that bring lasting value to communities.

### **General Development Plan**

#### **Layout & Design**

119 single family residential lots are proposed. The site plan and preliminary plat accommodate lots that are consistent with R-5 minimum zoning standards. Additional parameters exceeding municipal code are generally applied as outlined:

	<b>R-5 Zoning</b>	<b>Proposed Parameters</b>
<b>Minimum Lot Size</b>	6,000sf	8,700sf
<b>Minimum Lot Width</b>	-	70'
<b>Front Setback</b>	8'	25'
<b>Side Setback</b>	5' from either side, 12' total	7.5'

The proposed zoning characteristics allow for flexibility that provide housing options meeting expectations of today's homebuyers. These variations provide practical yard space and serve modern floor plans while still ensuring attainable price points are met. Furthermore, the proposed characteristics lead to the preservation of site-specific open space more than what traditional zoning would allow. Open space is methodically planned by clustering lots in a fashion that retains existing environmental features and adds permanent green space.

#### **Open Space**

NEUMANN DEVELOPMENTS, INC. \* N27 W24025 PAUL CT. SUITE 100 \* PEWAUKEE, WI 53072  
262-542-9200 \* FAX: 262-349-9324 \* NEUMANNDEVELOPMENTS.COM



The site layout accounts for existing physical features, including tree lines, slopes, and wetlands. Preserving these assets in perpetuity enhances the desirability of the neighborhood and protects high-quality fauna. Altogether, approximately 18 acres will be designated as permanent green space.

Wetlands and the Secondary Environmental Corridor (S.E.C.) as identified in the General Development Plan have been delineated by Eric Parker of Heartland Ecological Group, a professional assured delineator in Wisconsin. The delineation report outlines wetland indicators and has mapped areas accordingly. 1.21 acres are to be permanently protected based on the delineation results.

Outlot #4 consists of a 3.7 acre park to be dedicated to the City for public use. The park will be available to both current and future residents and provide passive and active recreational opportunities primarily serving the City's southern residents. Anticipated amenities include play equipment, sports courts, and walking trails.

Except for outlot #4, all outlots containing stormwater facilities, wetlands, landscaping, and green space would be fractionally owned by lot owners within the subdivision and privately managed by an incorporated homeowner's association (H.O.A.). Stormwater facilities would be accessible from right-of-way or access easements for maintenance purposes.

### **Specific Implementation Plan**

#### **Phasing Plan**

It is estimated Heritage Grove will be developed in four phases over a five-year period. Groundbreaking on the first phase will occur in summer of 2026, with substantial completion of infrastructure by late fall of 2026. The subdivision plat will be recorded to follow and legally create lots being conveyed to Harbor Homes, Inc. Homebuilding will begin early 2027, with first occupants expected by mid-spring of 2027.

Succeeding phases will follow a similar, seasonal construction cycle where sitewide improvements occur spring through fall for lot deliveries for homebuilding to commence the following year. An anticipated 20 homes per year will be completed for an eight-year total buildout, a sustainable building pace.

<b>Year</b>	<b>Phase</b>	<b>Estimated Lots Delivered</b>	<b>Estimated Homes Delivered</b>
2026	1	43	0
2027	-	-	5
2028	2	26	20
2029	3	18	20
2030	4	32	20
2031	-	-	20
2032	-	-	20
2033	-	-	14



## **Grading Plan**

The grading plan serves as a necessary component to ensure proper elevations are met for public improvements, stormwater flow paths, stormwater treatment facilities, and other conveyances. Contours provide existing and proposed topography for mass grading operations as the site is prepared for public and private infrastructure. The final master grading plan once complete will dictate yard grades for individual home construction to take place.

## **Utility Plan**

Infrastructure installation and subdivision platting will occur in an estimated four separate construction phases. Proposed improvements include municipal sanitary sewer, water, and storm sewer systems. Sanitary sewer will be gravity-fed and connect to existing sewer located within the Greystone subdivision, immediately north of the site. Water main would also connect to the existing system in Greystone at Trillium Avenue. Stormwater will be primarily managed on-site, with multiple discharges into planned retention basins. Approximately five wet basins would be built to account for stormwater capacity and water quality. The installation of private utilities would be coordinated with WPS and Plymouth Utilities.

## **Road Profile**

CTH E will be extended northward into the site and will serve as the primary ingress and egress for the subdivision. Future access points include a connection to the existing Trillium Avenue to the northwest, as well as a stub road at the northeast edge of the property that could serve potential future development northeast of the subject parcel.

The right-of-way profile applied in the development plan consists of the City's standard roadway specifications and engineering design standards. Right-of-way is identified on the plans and would ultimately be dedicated to the City for public use. The proposed road network provides sufficient access for public safety services and encourages efficient vehicular and pedestrian traffic flow serving both this subdivision and the surrounding area. Overall, the road strengthens both connectivity and walkability for the area, is consistent with municipal ordinances, and brings convenience to both current and future residents to utilize community amenities.

## **Architecture**

Protective covenants would be recorded and require architectural elements and private landscaping treatments in excess of municipal ordinances. The covenants define H.O.A. responsibilities and structure, and they enforce compliance of architectural standards which sustain valuations and maintain an attractive neighborhood aesthetic. One story homes must be a minimum of 1,600 square feet; two story homes must be a minimum of 1,800 square feet. Garages must be able to hold two vehicles and be 400 square feet minimum. In addition to home construction requirements, exterior alteration specifications would have to be approved by an Architectural Control Committee.



## **Phase 1 Implementation**

The first construction phase will extend the existing CTH E northward into the site with a residential road profile (“Founders Way”) and continue east to provide both primary access for the public park and secondary access for the adjacent multifamily parcel as soon as possible. Mass grading will involve earthwork for the public roadways, lots, utility extensions, and stormwater management. Stormwater facilities installed will ensure proper conveyance and treatment of runoff. Two treatment facilities will serve the watershed area of the first phase. Altogether, 43 lots and the multifamily building pad will be delivered in Phase 1.

Sanitary sewer installation would begin at an existing manhole within an easement of the Greystone subdivision and be extended into the site to the future Founders Way to service the first construction phase. Sanitary extensions to service planned multifamily development is also included with this phase. Additional sanitary installation outside of the Phase 1 roadway limits include (2) extensions within the future Pioneer Place roadway to allow for water main installation to follow above. Water main must be connected to the existing system at Trillium Avenue and run through the future Pioneer Place to reach the Phase 1 limits; it would then extend through the first phase and terminate at STH 67.

## **Outcome**

This petition is being made after careful consideration regarding the market demand in the Plymouth area. Along with the unique partnership with the City, we expect to bring in new members to the community and expand for-ownership options for years to come. The partnership allows for delivering expected price points in the low \$400s for home-lot packages, a financially feasible housing product in today’s market. New families will support businesses, increase the tax base, and enrich the community with energy and ideas. Some buyers are expected to be current City residents, thus freeing up existing housing and increasing the overall market supply.

Thank you for your consideration. We look forward to continuing to work with City of Plymouth on this generational opportunity.

Sincerely,

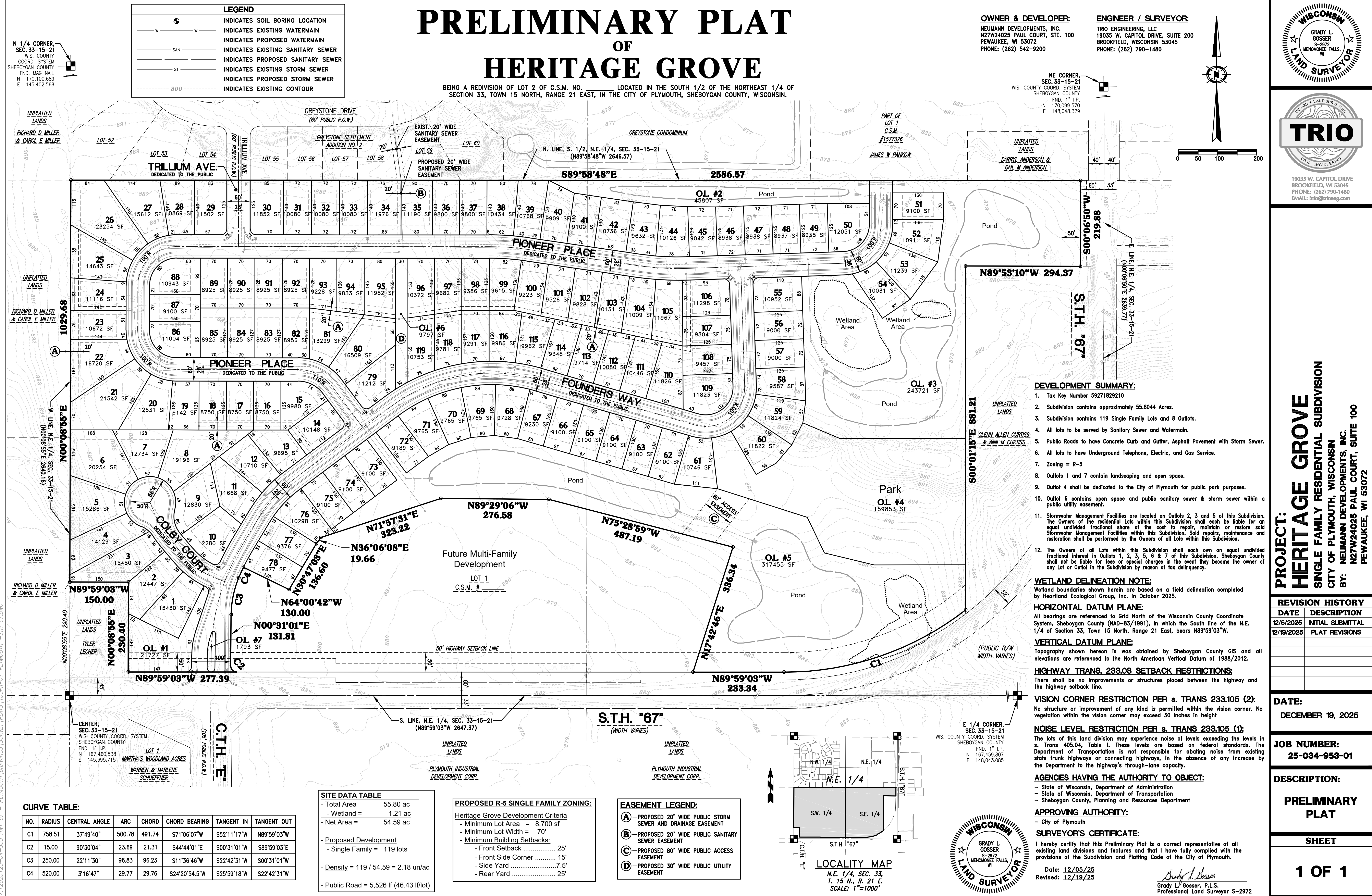
*Ryan Fritsch*

Ryan Fritsch

Neumann Developments, Inc.




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 CONTRACTOR IS REQUIRED TO CONTACT DIGGERS HOTLINE TOLL FREE TO OBTAIN LOCATION OF UNDERGROUND UTILITIES PRIOR TO COMMENCING THE WORK. WISCONSIN STATUTE 182.0715 REQUIRES MIN. OF 3 WORK DAYS NOTICE BEFORE YOU EXCAVATE.  
**CALL DIGGERS HOTLINE 1-800-242-8511**

**NOTE:** EXISTING UNDERGROUND UTILITY INFORMATION WAS OBTAINED FROM AVAILABLE RECORDS. THE ENGINEER MAKES NO GUARANTEE AS TO THE ACCURACY OF THIS INFORMATION. VERIFICATION TO THE SATISFACTION OF THE CONTRACTOR OF ALL UNDERGROUND UTILITIES, WHETHER OR NOT SHOWN ON THE PLANS, SHALL BE ASSUMED AS A CONDITION OF THE CONTRACT. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES BETWEEN LOCATION OF UTILITIES IN THE FIELD AND LOCATIONS SHOWN ON THE PLANS.

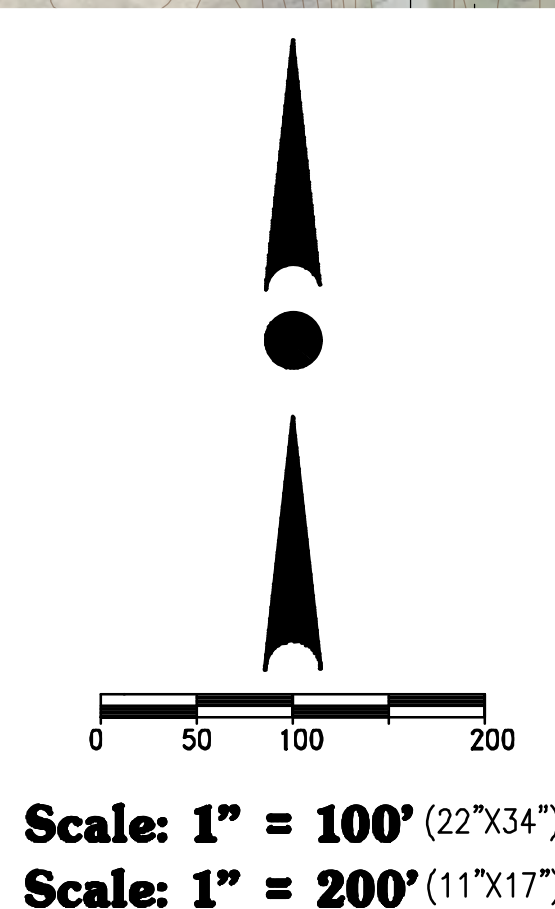
**PROPOSED R-5 SINGLE FAMILY ZONING:**

Heritage Grove Development Criteria

- Minimum Lot Area = 8,700 sf
- Minimum Lot Width = 70'
- Minimum Building Setbacks:
  - Front Setback ..... 25'
  - Front Side Corner ..... 15'
  - Side Yard ..... 7.5'
  - Rear Yard ..... 25'

**SINGLE FAMILY SITE DATA**

- Total Area 55.80 ac
- Wetland = 1.21 ac
- Net Area = 54.59 ac
- Proposed Development = 119 lots
- Density = 119 / 54.59 = 2.18 un/ac
- Public Road = 5,526 lf (46.43 lf/lot)





4100 N. CALHOUN RD., SUITE 300  
BROOKFIELD, WI 53005  
PHONE: (262) 790-1480  
FAX: (262) 790-1481  
EMAIL: info@trioeng.com

**PROJECT:**  
**HERITAGE GROVE**  
CITY OF PLYMOUTH, WISCONSIN

**BY:** NEUMANN DEVELOPMENTS, INC  
N27W24026 PAUL COURT, SUITE 100  
PEWAUKEE, WI 53072

REVISION HISTORY	
DATE	DESCRIPTION
12/6/2025	PRELIMINARY SET
12/18/2025	PRELIMINARY SET

**DATE:**  
DECEMBER 19, 2025

**JOB NUMBER:**  
25-34-953

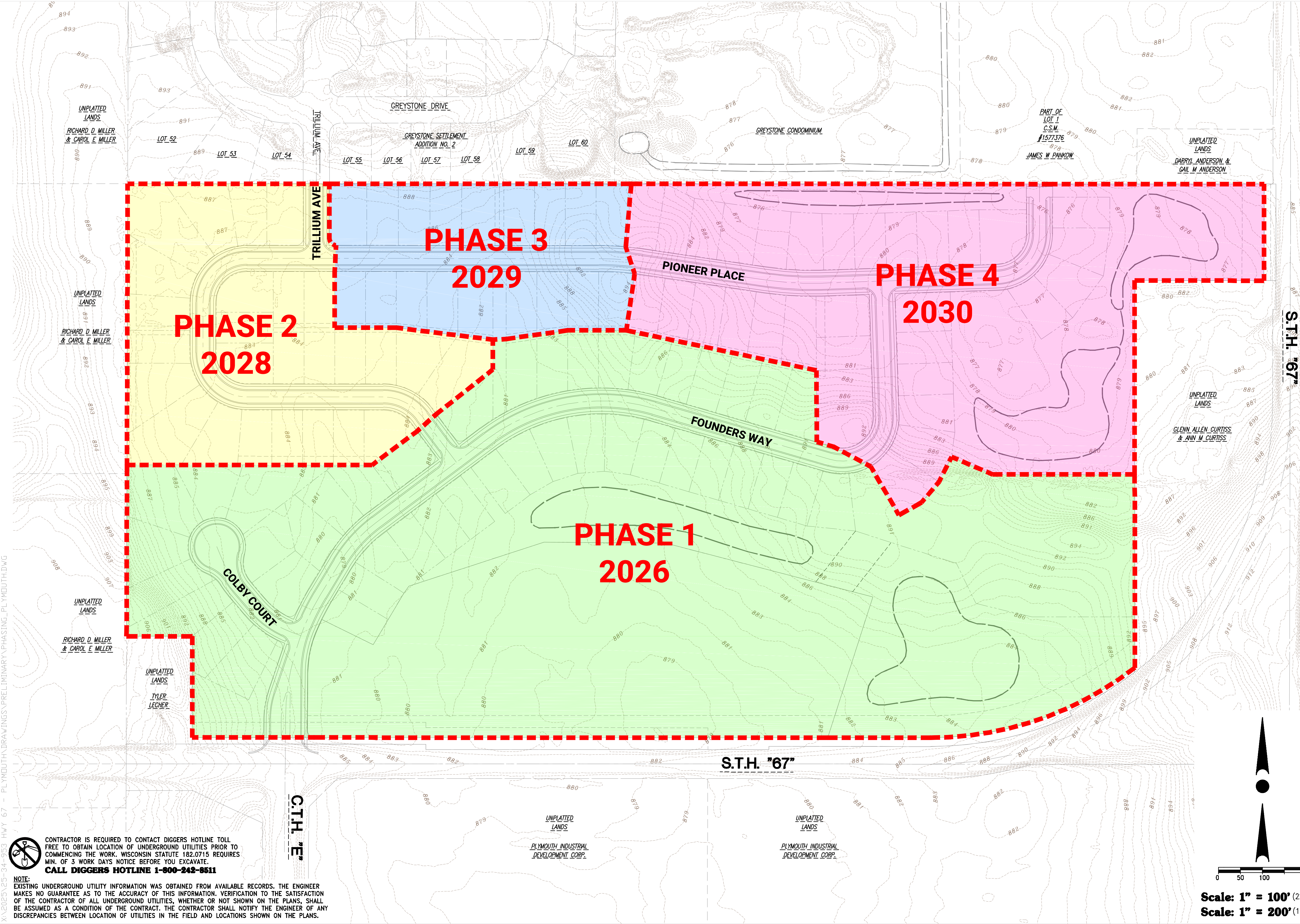
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**PROPOSED SITE PLAN**

**SHEET**

**C1.0**

Page 20 of 33



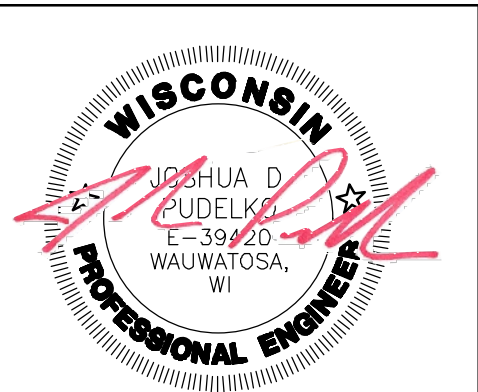


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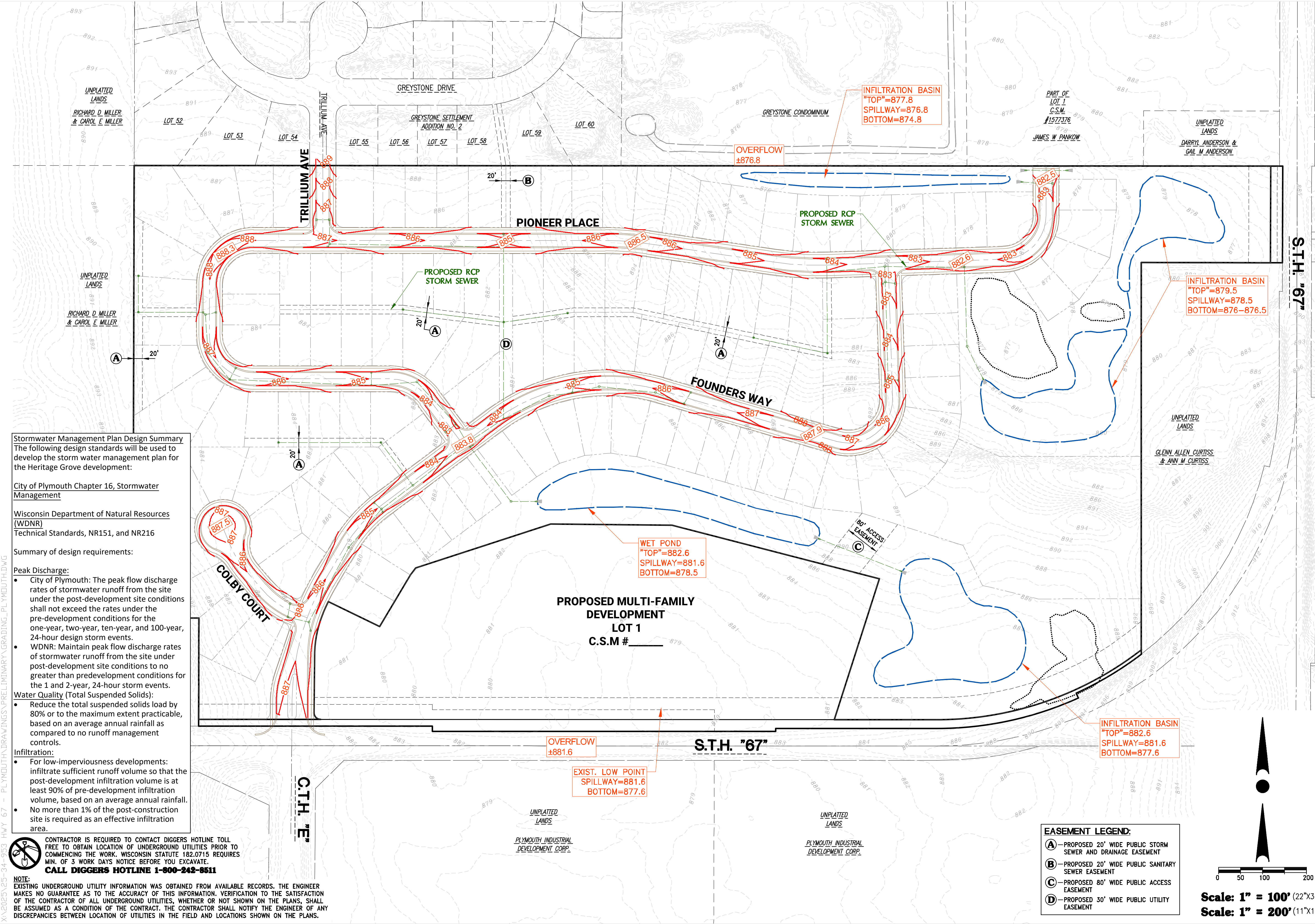
**JOB NUMBER:**  
25-34-953

**DESCRIPTION:**  
**PROPOSED PHASING PLAN**

**SHEET**

**C1.1**





**Stormwater Management Plan Design Summary**  
The following design standards will be used to develop the storm water management plan for the Heritage Grove development:

City of Plymouth Chapter 16, Stormwater Management

Wisconsin Department of Natural Resources (WDNR)  
Technical Standards, NR151, and NR216

Summary of design requirements:

**Peak Discharge:**

- City of Plymouth: The peak flow discharge rates of stormwater runoff from the site under the post-development site conditions shall not exceed the rates under the pre-development conditions for the one-year, two-year, ten-year, and 100-year, 24-hour design storm events.
- WDNR: Maintain peak flow discharge rates of stormwater runoff from the site under post-development site conditions to no greater than predevelopment conditions for the 1 and 2-year, 24-hour storm events.

**Water Quality (Total Suspended Solids):**

- Reduce the total suspended solids load by 80% or to the maximum extent practicable, based on an average annual rainfall as compared to no runoff management controls.
- Infiltration:**
  - For low-imperiousness developments: infiltrate sufficient runoff volume so that the post-development infiltration volume is at least 90% of pre-development infiltration volume, based on an average annual rainfall.
  - No more than 1% of the post-construction site is required as an effective infiltration area.

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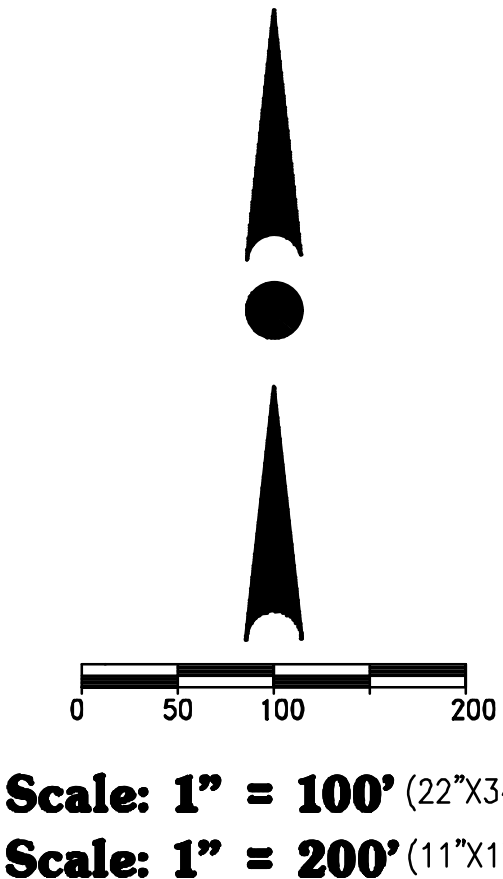
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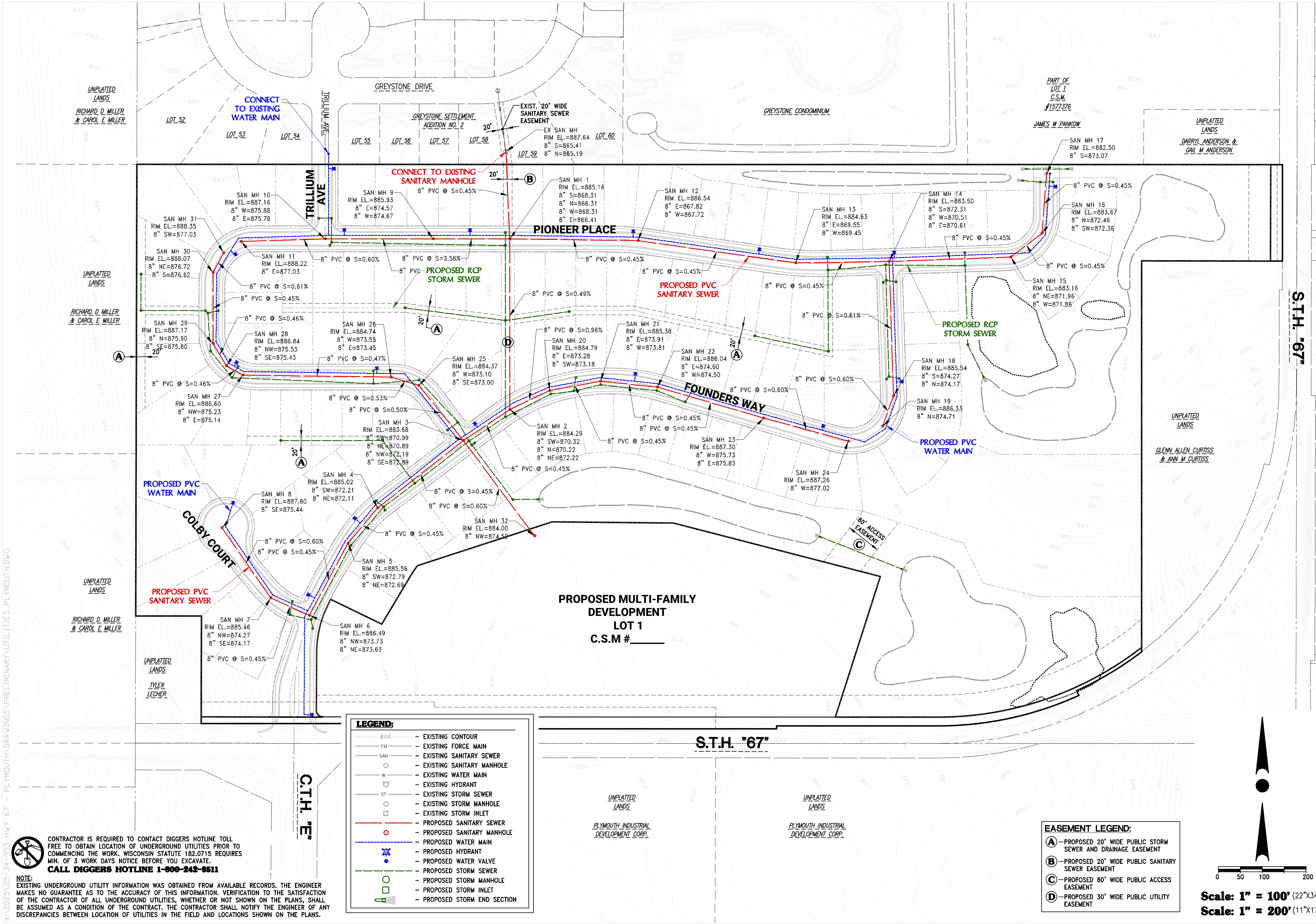
**DESCRIPTION:**  
OVERALL  
GRADING PLAN

**SHEET**  
**C2.0**

- EASEMENT LEGEND:**
- (A) — PROPOSED 20' WIDE PUBLIC SEWER AND DRAINAGE EASEMENT
  - (B) — PROPOSED 20' WIDE PUBLIC SANITARY SEWER EASEMENT
  - (C) — PROPOSED 80' WIDE PUBLIC ACCESS EASEMENT
  - (D) — PROPOSED 30' WIDE PUBLIC UTILITY EASEMENT







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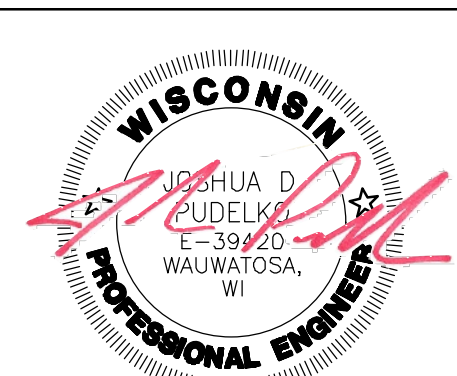
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**LEGEND:**

- |     |                            |
|-----|----------------------------|
| --- | EXISTING CONTOUR           |
| FM  | EXISTING FORCE MAIN        |
| SAN | EXISTING SANITARY SEWER    |
| ○   | EXISTING SANITARY MANHOLE  |
| W   | EXISTING WATER MAIN        |
| ○   | EXISTING HYDRANT           |
| ST  | EXISTING STORM SEWER       |
| ○   | EXISTING STORM MANHOLE     |
| □   | EXISTING STORM INLET       |
| --- | PROPOSED SANITARY SEWER    |
| ○   | PROPOSED SANITARY MANHOLE  |
| --- | PROPOSED WATER MAIN        |
| ○   | PROPOSED HYDRANT           |
| --- | PROPOSED WATER VALVE       |
| --- | PROPOSED STORM SEWER       |
| ○   | PROPOSED STORM MANHOLE     |
| □   | PROPOSED STORM INLET       |
| --- | PROPOSED STORM END SECTION |

**EASEMENT LEGEND:**

- (A) - PROPOSED 20' WIDE PUBLIC STORM SEWER AND DRAINAGE EASEMENT  
(B) - PROPOSED 20' WIDE PUBLIC SANITARY SEWER EASEMENT  
(C) - PROPOSED 80' WIDE PUBLIC ACCESS EASEMENT  
(D) - PROPOSED 30' WIDE PUBLIC UTILITY EASEMENT



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25-34-953

**DESCRIPTION:**  
**OVERALL  
UTILITY PLAN**

**SHEET**

**C3.0**



Jack Johnston  
Plan Commission Secretary  
City of Plymouth  
128 Smith Street  
Plymouth, WI 53073

January 13, 2026

RE: Heritage Grove Preliminary Plat Review, City of Plymouth

Jack,

We have reviewed the above noted preliminary plat, per Title 14 – Subdivision Regulations, Article E. – Technical Requirements for Plats and Certified Survey Maps, of the City of Plymouth Municipal Code, and have compiled the following comments:

**Sec. 14-1-40 – Technical requirements for preliminary plats.**

1. Must indicate, type, width and elevation of any existing street pavements within the exterior boundaries of the plat or immediately adjacent thereto, together with any legally established centerline elevations. [14-1-40(b)(5)]
2. Must indicate, location and size of any existing sanitary or storm sewers, culverts and drain pipes, the location of manholes, catch basins, hydrants, electric and communication facilities, whether overhead or underground and the location and size of any existing water and gas mains within the exterior boundaries of the plat or immediately adjacent thereto. If no sewers or water mains are located on or immediately adjacent to the tract, the nearest such sewers or water mains which might be extended to serve the tract shall be indicated by the direction and distance from the tract, size and invert elevations. [14-1-40(b)(6)]
3. Must indicate, corporate limit lines within the exterior boundaries of the plat or immediately adjacent thereto. [14-1-40(b)(7)]
4. Must indicate, existing zoning adjacent to the proposed subdivision. [14-1-40(b)(8)]
5. At least two permanent benchmarks shall be located in the immediate vicinity of the plat; the location of the benchmarks shall be indicated on the plat, together with their elevations referenced to Mean Sea Level Datum and the monumentation of the benchmarks clearly and completely described. Where, in the judgment of the city engineer, undue hardship would result because of the remoteness of the parcel from a mean sea level reference elevation, another datum may be used. [14-1-40(b)(9)]

6. Must indicate, high-water elevation of all ponds, streams, lakes, flowages and wetlands within the exterior boundaries of the plat or located within 100 feet therefrom. [14-1-40(b)(10)]
7. Must indicate, water elevation of all ponds, streams, lakes, flowages and wetlands within the exterior boundaries of the plat or located within 100 feet therefrom at the date of the survey. [14-1-40(b)(11)]
8. Location, width and names of all proposed streets and public rights-of-way such as alleys and easements. Indicate the width of proposed Colby Court. [14-1-40(b)(14)]
9. Approximate radii of all curves. Not all proposed curves include the radius. [14-1-40(b)(17)]

If you have any questions or concerns, please call or reply.

Regards,  
KAPUR & ASSOCIATES, INC.



Erik A. Gustafson, P.L.S.  
Project Surveyor

Ms. Cathy Austin, P.E.  
City of Plymouth  
900 CTH PP – P.O. Box 277  
Plymouth, WI, 53073

28 January 2026

From: Neal Styka, P.E., PTOE, RSP1 - Kapur & Associates, Inc.  
CC: Cathy Austin, P.E. – Public Works Director/City Engineer - City of Plymouth  
Jack Johnston - Assistant City Administrator/Community Development Director – City of Plymouth  
Aaron Groh, P.E. - Kapur & Associates, Inc.

RE: Residential Development Traffic Analysis (Neumann Development)

Dear Cathy:

Kapur has reviewed the traffic impact analysis (TIA), for the new multifamily and single-family development that is proposed on the north and west side of State Trunk Highway 67 (STH 67) south of the existing Greystone Settlement neighborhood in the City of Plymouth.

This is the TIA for the Initial Review Letter that was submitted on September 23, 2025. The review of this Residential Development Traffic Analysis is based on the Institute of Transportation Engineers (ITE) Trip Generation Manual, 11th Edition and the Wisconsin Department of Transportation (WisDOT) Traffic Impact Analysis Guidelines most recent update of April 2024.

**Documents Submitted:**

Residential Development Traffic Analysis dated October 16, 2025.

Revised Residential Development Traffic Analysis dated January 26, 2026

**Review Comments:**

Previously submitted comments have been successfully addressed.

**Conclusions:**

The on-site residential development is expected to be completed around the year 2027 for 60 multifamily homes (5 buildings) and around 2033 for 119 single family homes. It is expected to have one access driveway on STH 67 opposite of CTH E and one access connection to the existing Greystone Settlement via Trillium Avenue.

The TIA Initial Review defines the Peak Hours as

- Weekday AM Peak hour: 7:00 – 8:00 am
- Weekday PM Peak Hour: 3:30 – 4:30 pm

And based on the trip generations from the ITE *Trip Generation Manual, 11<sup>th</sup> Edition* it will generate:

- Weekday Daily: 1,640 new trips
- Weekday AM Peak Hour: 125 new trips (30 in/95 out)
- Weekday PM Peak Hour: 160 new trips (100 in/60 out)



The distribution is shown as:

- 10% to/from the west on STH 67
- 10% to/from the south on CTH E
- 35% to/from the north on STH 67
- 35% to/from the east on CTH E/PP
- 10% to/from the west on Hill and Dale Road

This distribution shows most of the traffic will head east on CTH E/CTH PP and North on STH 67 towards Plymouth STH 57 and STH 23. This conclusion that most of the traffic will head in those directions is consistent with places of employment within the City of Plymouth and provides access to major roadways.

Currently no changes at the intersection of STH 67 & Hill and Dale Road/CTH E/PP are recommended. However, if more developments occur in this area, then changes to pavement marking and signing are recommended. The northbound and southbound approaches currently have one lane with a bypass lane that has no pavement marking arrows or signing denoting which movements are allowed in the lanes. In the future it is recommended to mark and sign the southbound approach with one left-turn lane, and one shared through/right-turn lane. For the northbound approach in the future, it is recommended to mark and sign the approach as a shared left-turn/through lane and one right-turn lane. This intersection, according to the study, currently meets the thresholds in the MUTCD and the WisDOT criteria to be considered for all-way stop control. This would be installed at the discretion of the WisDOT. A Phase 1 Intersection Control Evaluation which analyzes different traffic control methods is required to be completed before all-way stop control is considered. Additionally, The WisDOT prefers the minimal use of all-way stop control as a permanent traffic control method because it believes in a concept of maintaining mobility by allowing traffic to “free-flow” as much as possible.

The capacity of CTH PP between STH 67 and STH 57 should be evaluated with this development and other developments that are planned in the City.

On the south side of the development the proposed access location is recommended to be opposite CTH E with one southbound shared right-turn/through/left-turn lane with stop control. The westbound approach will need to be widened so that the westbound approach has one 350' long left-turn lane with a 150' long taper, one through lane, and one 350' long right-turn lane with a 150' foot taper. The eastbound approach will have one shared left-turn/through lane and one right-turn lane. The northbound approach should have one shared left-turn/through lane and one right-turn lane. While turn lanes for eastbound approaches is not warranted there are bypass lanes that the TIA recommends restriping and signing.

Please call (414) 751-7238 or email me at [nstyka@kapurinc.com](mailto:nstyka@kapurinc.com) if you have any questions or comments regarding this review.

Sincerely,  
Kapur

Neal Styka, P.E., PTOE, RSP1